



THIS MEETING WILL BE WEBCAST ON THE [CITY'S PUBLIC YOUTUBE SITE](#) (CITYWATERLOO) AND MAY BE TELECAST ON PUBLIC TELEVISION



## **COUNCIL MEETING AGENDA – **REVISED****

Monday, September 8, 2025

Closed Meeting: 2:00 PM

Public Meeting: 3:00 PM

Councillor Bodaly in the Chair

**1. DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF**

**2. CLOSED MEETING**

**Recommendation:**

That Council hold a closed meeting for the purposes of considering the following subject matter:

- a) personal matters about an identifiable individual, including municipal or local board employees (Potential Disposition of Lands)
- b) advice that is subject to solicitor-client privilege, including communications necessary for that purpose (Claims Update, Potential Disposition of Lands);
- c) litigation or potential litigation, including matters before administrative tribunals, affecting the municipality or local board (Claims Update, Potential Disposition of Lands); and
- d) a proposed or pending acquisition or disposition of land by the municipality or local board (Potential Disposition of Lands).

**COUNCIL MEETING WILL RECESS AND  
RECONVENE AT 3:00 PM**

3. **TERRITORIAL ACKNOWLEDGEMENT**
4. **MOMENT OF REFLECTION**
5. **DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF**
6. **APPROVAL OF MINUTES**

That the previous meeting minutes be approved.

**a) June 9, 2025 – Council Meeting**

**Recommendation:**

That the minutes of the Council meeting held on June 9, 2025 be approved as printed.

**b) June 16, 2025 – Council Meeting**

**Recommendation:**

That the minutes of the Council meeting held on June 16, 2025 be approved as printed.

**c) June 23, 2025 – Council Meeting**

**Recommendation:**

That the minutes of the Council meeting held on June 23, 2025 be approved as printed.

**d) July 14, 2025 – Council Meeting**

**Recommendation:**

That the minutes of the Council meeting held on July 14, 2025 be approved as printed.

**7. DELEGATIONS**

**a) Sustainable Waterloo Region**

Tova Davidson, Executive Director, Sustainable Waterloo Region



## 8. CONSENT MOTION

That consent motion item (a) be approved.

- a)     **Title:**               **Fireworks By-law Amendment**  
Report No.:   COM2025-021  
Prepared By:  Grant Curlew

**Recommendation:**

1.     That Council approve report COM2025-021.
2.     That Council approve amendments to the Fireworks By-law #2025-030.

## 9. ITEMS REMOVED FROM THE CONSENT MOTION

## 10. STAFF REPORTS

- a)     **Title:**               **HAF Sugarbush South Corridor** **Page 9**  
                                  **Expansion Initiative – Draft Official Plan**  
                                  **Amendment, Zoning By-law Amendment**  
                                  **and Urban Design Guidelines**  
Report No.:   IPPW2025-036  
Prepared By:  Brenna MacKinnon

**Presentation:** Richard Kelly-Ruetz, Senior Planner, GSP Group

**Recommendation:**

1.     That report IPPW2025-036 be received for information.
2.     That Council direct staff to consult with the public and interested parties on the draft Official Plan Amendment, Zoning By-law Amendment and complementary Urban Design Guidelines for Sugarbush South.

- b)     **Title:**               **Lexington Road Complete Streets Study**  
Report No.:   IPPW2025-033  
Prepared By:  Chris Hodgson

**Presentation:** Christopher Hodgson

**Delegation:**

1.     Vincent Russo and Jovana Russo, Residents of Waterloo

**Recommendation:**

1. That Report IPPW2025-033 be approved.
2. That the Preferred Plan contained in Report IPPW2025-033 for Lexington Road be approved.
3. That Staff be authorized to progress the plan to detailed design phase to meet the timelines of the Federal Active Transportation Fund program.

**c) Title: Multi-Unit Residential Acquisition Pilot Program (MURA)**

Report No.: IPPW2025-048

Prepared By: Adam Zufferli and Douglas Stewart

**Presentation:** Adam Zufferli and Douglas Stewart

**Recommendation:**

1. That Council approve IPPW2025-048.
2. That Council approve the Multi-Unit Residential Acquisition Pilot Program, attached as Appendix 'A' to IPPW2025-048.
3. That Council direct staff to develop and publish the implementation materials for the Multi-Unit Residential Acquisition Pilot Program.

**11. PUBLIC MEETINGS**

**Formal Public Meeting**

**a) Title: Zoning By-law Amendment  
Z-25-07, 380 Hagey Boulevard**

Report No.: IPPW2025-053

Prepared By: Kaitlyn Bisch

Ward No.: 6 – Central Columbia

**Presentation:** Kaitlyn Bisch

**Delegations:**

1. Adam Layton, Associate Principal, Goldberg Group

**Recommendation:**

1. That council approve report IPPW2025-053
2. That Council approve Zoning By-law Amendment Z-25-07, 380 Hagey Boulevard, in accordance with Section 7 of staff report IPPW2025-053

## **COUNCIL MEETING WILL RECESS AND RECONVENE AT 6:30 PM**

12. **DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF**
13. **PUBLIC MEETINGS continued**

### **Formal Public Meeting**

- b)     **Title:**                   **Official Plan Amendment 52,  
Zoning By-law Amendment Z-24-03,  
177-179 Albert Street, 14688210 Canada Inc.**
- Report No.:   IPPW2025-025  
Prepared By:   Max Kerrigan  
Ward No.:       7-Uptown Ward

**Presentation:**       Max Kerrigan

### **Delegations:**

1.     Pierre Chauvin, Partner, MHBC Planning  
       Kate Wills, Associate, MHBC Planning

2.     Sharon Woodley, Resident of Waterloo

### **Recommendation:**

1.     That Council approve report IPPW2025-025.
2.     That Council approve Official Plan Amendment 52 (OPA 52), 177-179 Albert Street, in accordance with Section 7 of report IPPW2025-025.
3.     That Council approve Zoning By-law Amendment Z-24-03, 177-179 Albert Street, in accordance with Section 7 of report IPPW2025-025.

**c) Title: Zoning By-law Amendment Z-24-17  
for 137 & 141 Woolwich Street**

Report No.: IPPW2025-051

Prepared By: Aminu Bello

Ward No.: Southeast, Ward 5

**Presentation:** Aminu Bello

**Delegations:**

1. Michael Witmer, Senior Planner, GSP Group
2. Naz Rahimi-Mirzaagha, Resident of Waterloo
3. Saba Mirzaagha, Resident of Waterloo
4. Abhijit Shukla and Falguni Shukla, Residents of Waterloo
5. Kerry Church, Resident of Waterloo
6. Kim Cyr, Resident of Waterloo

**Recommendation:**

1. That Council approve report IPPW2025-051.
2. That Council approve Zoning By-law Amendment Z-24-17, 1000686403 Ontario Ltd. (Opulence Homes), for lands municipally known as 137 and 141 Woolwich Street, in accordance with Section 7 of Staff Report IPPW2025-051.

**14. CONSIDERATION OF NOTICE OF MOTION GIVEN AT PREVIOUS MEETING**

None

**15. NOTICE OF MOTION**

None

**16. COMMUNICATIONS AND CORRESPONDENCE**

None

**17. UNFINISHED BUSINESS**

None

**18. QUESTIONS**

**19. NEW BUSINESS**

**a) September 22, 2025 Closed Meeting Location**

On September 22, 2025 Council will attend a closed meeting to receive education and training at Waterloo Park in Waterloo. In accordance with the Procedure By-Law, Council must approve the holding of a meeting of council at an alternate location. The public portion of the meeting will begin with a motion to go into a closed session. Therefore, the majority of the session will be in a closed session, not open to the public. The public meeting will resume at 4:00 p.m. in the Waterloo City Centre, Council Chambers.

**Recommendation:**

That Council approve the location for the 1:00 p.m. portion of the Meeting of Council to be held on Monday, September 22, 2025 at Waterloo Park, Waterloo, in accordance with Section 236 (1) of the Municipal Act, 2001 and section 2.05 of the City of Waterloo Procedure By-law, 2019-057. The purpose of the meeting will be to move the motion to go into a closed session for education and training purposes.

**20. ENACTMENT OF BY-LAWS**

**Recommendation:**

That the By-laws listed below be read a first, second and third time and finally passed, numbered sequentially commencing with By-law Number 2025-054 and that the Mayor and Clerk be authorized to sign them accordingly.

- a) By-law to Amend City of Waterloo By-law #2025-030 (COM2025-021, Council September 8, 2025)
- b) By-Law to Amend By-law No. 2018-050, being a Zoning By-law Controlling Land Use Development within the City of Waterloo for 137 and 141 Woolwich Street (IPPW2025-051, Council September 8, 2025)
- c) By-Law to Amend Statutory Officer Appointment By-Law(s)

- d) By-law to confirm all actions and proceedings of Council,  
September 8, 2025

## **21. ADJOURNMENT**



## STAFF REPORT Planning

Title: HAF Sugarbush South Corridor Expansion Initiative – Draft Official Plan Amendment, Zoning By-law Amendment and Urban Design Guidelines

Report Number: IPPW2025-036

Author: Brenna MacKinnon

Council Date: September 8, 2025

File: Housing Accelerator Fund

Attachments: Appendix A – Planning Brief  
Appendix B – Draft Official Plan Amendment  
Appendix C – Draft Zoning By-law Amendment  
Appendix D – Draft Urban Design Guidelines

Ward No.: 6 – Central-Columbia Ward

### Recommendations:

1. That report IPPW2025-036 be received for information.
2. That Council direct staff to consult with the public and interested parties on the draft Official Plan Amendment, Zoning By-law Amendment and complementary Urban Design Guidelines for Sugarbush South.

### A. Executive Summary

In 2024, the City received a Housing Accelerator Fund (“HAF”) grant to implement eight housing initiatives. These initiatives are all focused on increasing the supply of housing and/or streamlining planning processes. Disbursement of the HAF grant monies is dependent on the achievement of specific milestones and the issuance of 650 building permits by 2027 that are over and above the City’s historical housing growth.

This report relates to HAF Initiative 3: the Sugarbush South Land Use Plan, being undertaken with the support of GSP Group to enable missing middle housing in the neighbourhood. The report provides an overview of the Planning Brief, draft Official Plan Amendment, Zoning By-law Amendment and Urban Design Guidelines to facilitate a medium rise residential framework in the neighbourhood. It also outlines the engagement strategy to solicit feedback on the drafts. A final land use plan is anticipated to be presented to Council by March 31, 2026. Enabling missing middle housing in the Sugarbush South neighbourhood could help the City achieve its Provincial housing target of 16,000 new homes by 2031, and its overall HAF commitment.

### **B. Financial Implications**

This initiative is being funded by the Housing Accelerator Fund grant. All costs associated with the initiative and implementation will be tracked for reimbursement from the HAF grant proceeds.

### **C. Technology Implications**

None.

### **D. Link to Strategic Plan**

(Strategic Priorities: Reconciliation, Equity, Accessibility, Diversity and Inclusion; Environmental Sustainability and Climate Action; Complete Community; Infrastructure and Transportation Systems; Innovation and Future-Ready)

(Guiding Principles: Equity and Inclusion; Sustainability; Integrity; Workplace Wellbeing; Community-centred; Operational Excellence)

**Complete Community:** This initiative will enable more “missing middle”, transit supportive housing options in proximity to the City’s three post-secondary institutions. This initiative is one of the actions in the City’s Affordable Housing Strategy and will help the City achieve its Housing Accelerator Fund commitment of 650 additional units over and above the City’s historic growth rate.

### **E. Previous and Related Reports on this Topic**

IPPW2025-001 – Housing Accelerator Fund Sugarbush South Planning Framework  
IPPW2024-032 – Housing Accelerator Fund: Corridor Expansion Study & Plan  
Initiative CAO2024-014 – Housing Accelerator Fund Launch and Progress Reporting  
IPPW2023-037 – Housing Accelerator Fund: City of Waterloo Application





### **HAF Sugarbush South Land Use Planning Framework– Draft Official Plan Amendment, Zoning by-law Amendment and Urban Design Guidelines IPPW2025-036**

#### **1.0 Background**

To help address the housing supply challenges facing communities across Ontario and Canada, governments at all levels are looking at ways to enable more housing. The expansion of the Columbia Street West Minor Corridor and enabling “missing middle”, transit supportive housing in the Sugarbush South neighbourhood, near the City’s post-secondary institutions, is an initiative in Waterloo’s CMHC-approved Housing Accelerator Fund (“HAF”) Action Plan.

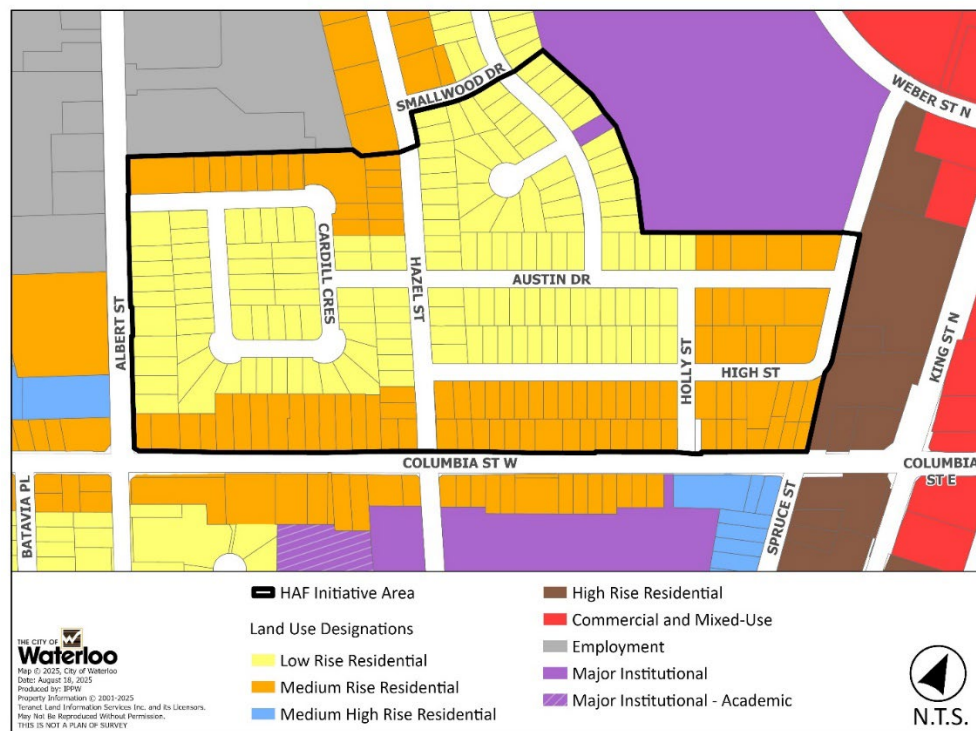
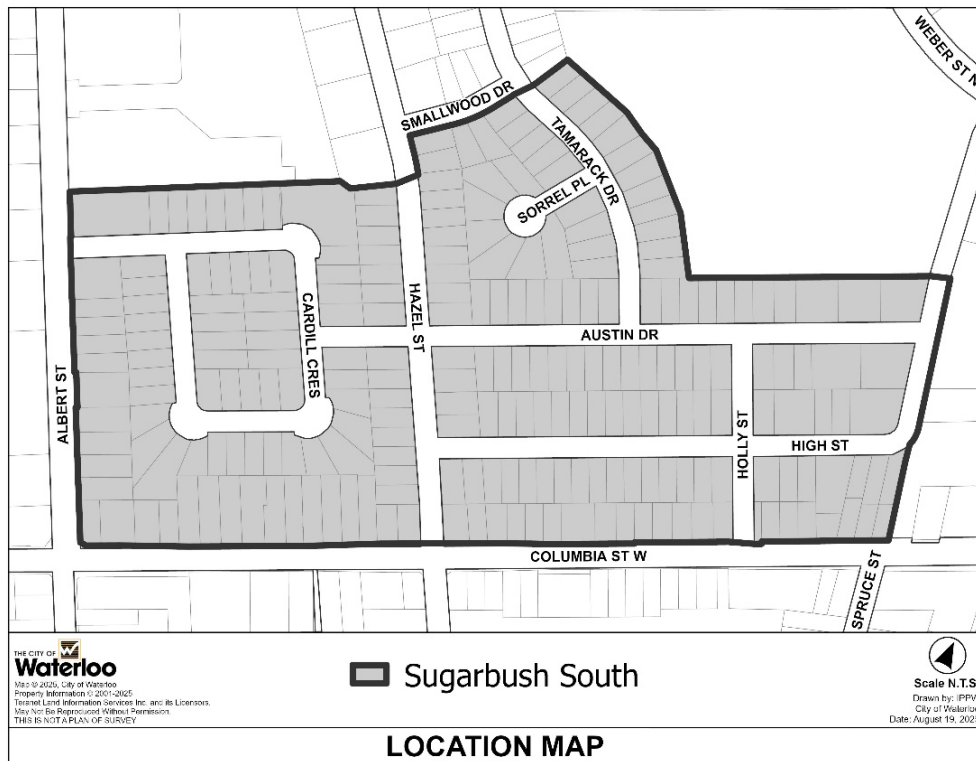
This initiative is an important step to help Waterloo achieve its Provincial housing pledge and the overall building permit requirements in the HAF grant conditions. The City’s Affordability Housing Strategy (IPPW2023-002) identified housing gaps such as the need for rental and ownership units within medium density “missing middle” built forms along transit routes and near amenities. For this initiative, missing middle housing refers to buildings of three to eight storeys such as stacked townhouses and mid-rise apartments.

#### **2.0 Initiative Overview**

Sugarbush South is ideally located for missing middle housing due to its proximity to two ION Station Areas, three post-secondary institutions, adjacency to Columbia Street West, and proximity to many services and amenities. Enabling missing middle housing in the Sugarbush South neighbourhood could help the City achieve its Provincial housing target of 16,000 new homes by 2031, and its overall HAF commitment of 650 additional units over and above the City’s historical growth rate.

Sugarbush South is bounded by Columbia Street West to the south, Albert Street to the west, Smallwood Drive to the north, and High Street to the east (see Figure 1 Location Map). Through OPA 58, the Columbia Street West Minor Corridor was expanded to include the north side of Columbia Street West (between Albert Street and west of the King Street North Major Corridor) and given a Medium Rise Residential designation. As shown in Figure 2, the neighbourhood is primarily designated “Low Rise Residential” in the interior and “Medium Rise Residential” along Columbia Street West, in the Major Transit Station Area (MTSA) on the north of Cardill Street, and in the area east of Holly Street (Official Plan Schedule ‘A’ – Land Use).

**Figure 1 Location Map**



### 3.0 Overview of the Sugarbush Land Use Plan

The City is working with GSP Group have advanced a Planning Brief, draft Official Plan Amendment, Zoning By-law Amendment and Urban Desing Guidelines to facilitate a medium rise planning framework in the neighbourhood. Work to date has taken into consideration the key project parameters identified in Council report IPPW2025-001 (informed by previous public engagement, agency comments and staff review) including:

#### a) Mix of unit types and opportunities for mixed-use development

As part of the Sugarbush South Land Use Plan, all residential lands in the neighbourhood are redesignated to Medium Rise Residential in the Official Plan. The Official Plan states that the aim of this designation is to encourage “*medium density, mixed use, primarily residential development within Minor Corridors, recognizing that properties may also function as a transition from low rise areas to medium-high and high-rise areas*”. The primary objective of permitting Medium Rise Residential permissions in the Sugarbush South neighbourhood is to expand the range of housing options, particularly missing middle housing such as townhouses and up to six to eight-storey multi-unit residential buildings.

Convertible Frontage areas have been added along Albert Street, Columbia Street West and portions of Cardill Crescent, Hazel Street and Holly Street where these streets intersect with Albert Street and Columbia Street. Convertible Frontage will enable the introduction of mixed uses over time as the community evolves.

The interior streets of Sugarbush South have been identified as Neighbourhood Frontage Areas. Building setbacks are larger than those in Northdale to provide greater protection for the existing mature tree canopy and space for utility and service providers.

#### b) Built form, to predominantly medium rise

Two new zoning categories have been developed for Sugarbush South – Residential Sugarbush 6 (RS-6) and Residential Sugarbush 8 (RS-8). Lands within the Minor Corridors (Columbia Street West and Albert Street) and Major Transit Station Area (north edge of Cardill Crescent) will be zoned RS-8 and permitted to have a building height of up to eight-storeys and a maximum density of 450 bedrooms per hectare. Similar to Northdale, the density can be increased up to 600 bedrooms per hectare with “bonusing” provisions that may apply through a Zoning By-law Amendment.

Lands located in the interior of the Sugarbush South neighbourhood will be zoned RS-6 and to allow building heights of up to six-storeys and a maximum density of 450 bedrooms. The density can be increased up to 600 bedrooms per hectare with “bonusing” that may apply through a Zoning By-law Amendment. These areas include portions of Cardill Crescent, Hazel Street, High Street, Austin Drive, Tamarack Drive, Sorrel Place, and Smallwood Drive.

c) Preservation/enhancement of the natural environment (e.g. trees)

The neighbourhood contains an existing mature tree canopy, typical in low rise residential neighbourhoods. As the neighbourhood redevelops, efforts will be made to preserve the existing tree canopy to the extent possible or to maximize opportunities for new trees or other landscaping measures.

b) Parkland

Flexible sites for parkettes have been identified in the Urban Design Guidelines, generally located at high visibility corners or where walkways converge. The parkette locations are intended to meet the needs of persons of all ages and abilities and foster social interaction within Sugarbush South.

c) Active transportation

A number of pedestrian walkways have been identified and are intended to improve pedestrian and community connectivity for current and future residents and visitors to the neighbourhood.

d) Urban design

The Sugarbush South Urban Design Guidelines have been prepared in support of the land use plan to provide a framework tailored to the neighbourhood as it redevelops with land use transitions and built forms consistent with the policies the City's Urban Design Manual.

e) Affordable housing

As noted in the Planning Brief, Sugarbush South has a varied demographic and plays an important role in providing rental housing. It is recognized that Sugarbush South contains rental housing that may be more affordable than other housing available in the City due to its form, tenure, location and age. To protect these assets from being lost to redevelopment and tenants displaced, a Holding symbol may be placed on lands through a zoning by-law amendment limiting building height to six storeys until such time as the City passes a rental replacement by-law.

The determination of an appropriate parking rate for Sugarbush South is an additional parameter requested by staff. GSP Group is working with Paradigm Transportation Solutions to provide an assessment based on other similar medium rise projects. Until that work is completed, a parking rate of 1.1 spaces per dwelling (1 space per unit plus 0.1 spaces per unit for visitor parking) is included in the RS-6 and RS-8 zones.

Provided below is a summary of the Planning Brief, specific policy and zoning regulation revisions and the Sugarbush South Urban Design Guidelines:

### **a) Planning Brief (Appendix A)**

The Planning Brief provides an overview of the key directions of the Sugarbush South Land Use Plan and explains the purpose of its individual components – the Official Plan Amendment, Zoning By-law Amendment, and Sugarbush South Urban Design Guidelines. It also provides a summary of how the Sugarbush South Land Use Plan considers the applicable Provincial, Regional, and City land use planning policy framework.

### **b) Draft Official Plan Amendment (Appendix B)**

#### **New Area Specific Policies:**

- Area Specific Policy 89 (Sugarbush South) is proposed to be added, which will contain the new policies for Sugarbush South. This policy will include the following subset policy:
  - Area Specific Policy 89A – policies for the existing medium rise apartments on Austin Drive and High to permit up to 8 storeys subject to the City enacting a rental replacement by-law (or equivalent in effect).

#### **Amendments to Official Plan Schedules:**

- Amend Schedule 'A' Land Use Plan to apply the Medium Rise Residential designation to the Sugarbush South area;
- Amend Schedule 'A6a' Area Specific Policies 20 and 45 to:
  - (1) Expand boundary to include Sugarbush South; and,
  - (2) Identify the following Area Specific Policies:
    - Area Specific Policy 89– applies to Sugarbush South area;
    - Area Specific Policy 89A – applies to the existing medium rise apartments on Austin Drive and High Street;
- Amend Schedule 'A45a' Street Frontage Areas and Streetscape Elements to include Sugarbush South;
- Amend Schedule 'B' City Structure to expand the Minor Corridor to Albert Street within Sugarbush South;
- Amend Schedule 'B1' Built Form to apply Medium Rise to Sugarbush South;
- Amend Schedule 'F' Active Transportation Framework to identify the conceptual walkways in Sugarbush South; and
- Amend the Area Specific Policies Appendix to add:
  - Area Specific Policy 89 – applies to Sugarbush South area;
  - Area Specific Policy 89A – applies to medium rise apartments on Austin Drive and High Street.

### **C) Draft Zoning By-law Amendment (Appendix C)**

#### **Section 3 – General Regulations**

- Amend Section 3.S.7 of Zoning By-law 2018-050 to expand the Street Frontages inset map to include proposed Street Frontages within Sugarbush South

(Convertible Frontages on Columbia Street and Albert Street and Neighbourhood Frontages in the interior of Sugarbush South).

## **Section 7 – Residential Zoning Categories**

Residential Sugarbush Six (RS-6) and Residential Sugarbush Eight (RS-8)

- Amend Section 7 to create a new Residential Sugarbush Six (RS-6) zone and new Residential Sugarbush Eight (RS-8) zone; and
- Apply a Holding Symbol (H) to all properties to restrict the use/development of lands until certain conditions of development are satisfied. In Sugarbush South, removal of these Holding Provisions would require verification of sufficient servicing capacity to fully service the lands, among any other conditions as deemed appropriate.

## **Schedule A – Zoning Map**

- Amend Zoning Map Schedule 'A' to update proposed zoning categories in Sugarbush South:
  - (H)RS-8 – applies to the Minor Corridor on Columbia Street and Albert Street, the Major Transit Station Area (MTSA) lands within Sugarbush South, and existing medium density lands on High Street and Austin Drive;
  - (H)RS-6 – applies to the balance of Sugarbush South.

## **Schedule C – Site Specific Zoning**

- Delete Exception C68 from the Tamarack Drive and Sorrel Place area which prohibits duplex, semi-detached and freehold semi-detached buildings given Council's recent approval of 4 units, 4 storeys;
- Add new Site Specific Exception C288 which applies to the existing medium rise apartments on Austin Drive and High Street RS-8 lands and has the following effect:
  - The Exception adds a holding provision (H) to limit building height to 21.5 metres (6 storeys) until a rental replacement by-law is in effect.

## **Schedule C1 – Site Specific Zoning**

- Delete Exception C68 from the Tamarack Drive and Sorrel Place area.
- Add new Site Specific Exception C288 which applies to the medium rise apartments on Austin Drive and High Street RS-8 lands.

## **d) Draft Sugarbush South Urban Design Guidelines (Appendix D)**

The Sugarbush South Urban Design Guidelines provide a set of objectives tailored to the Sugarbush South neighbourhood. These principles recognize the unique characteristics of the area, including the existing tree canopy and existing overhead hydro infrastructure in some areas of the neighbourhood. Proposed road widenings will need to be accommodated as redevelopment occurs on Albert Street, Columbia Street West, and Hazel Street in accordance with Official Plan Schedule 'G' – Road Allowances. Transportation Services staff have indicated that the identified road

---

widening on High Street (between Holly and Weber) and Holly Street (between Columbia Street West and High Street) is no longer needed.

#### **4.0 Engagement**

A formal Project Team has been established with staff from Planning, Parks, Engineering and Transportation along with members of GSP Group. Continued engagement with Enova Power Corporation has led to several draft zoning regulations regarding minimum setback requirements from buildings to overhead power lines as well as padmount transformer locations / access.

There will be several opportunities for the public, development industry, and other interested parties to engage on the draft Official Plan, Zoning By-law Amendment and Urban Design Guidelines. Methods of engagement will include:

- Engage Waterloo project page;
- Project email contact list;
- Property specific mail-out;
- In-person neighbourhood meeting (October 1);
- Virtual neighbourhood meeting (October 7);
- City's social media platforms; and,
- Newspaper ads.

#### **5.0 Next Steps**

This HAF initiative will help the City enable much needed missing middle, transit supportive housing in proximity to the post-secondary institutions and two light rail transit stations, to help address housing supply challenges. This HAF initiative is anticipated to support an estimated 4,800 additional bedrooms. The potential yield does not take into consideration additional development which may be achieved through 'bonusing' provisions. Through IPPW2025-036, staff have worked with GSP Group to advance a Planning Brief, draft Official Plan Amendment, Zoning By-law Amendment and Urban Design Guidelines to facilitate a medium rise planning framework in the neighbourhood.

The next step will be to solicit feedback from the neighbourhood, agencies and other interested parties such as developers on the drafts in September and October. A final land use plan is anticipated to be presented to Council by March 31, 2026.

# Planning Brief

---

## Sugarbush South – Draft Land Use Plan

City of Waterloo

Official Plan Amendment

Zoning By-law Amendment

Urban Design Guidelines

August 2025





# Contents

1.	Introduction.....	1
2.	Sugarbush South Land Use Plan.....	3
2.1	Overview of the Sugarbush Land Use Plan .....	3
2.2	Components of the Sugarbush Land Use Plan .....	6
3.	Policy Summary.....	9
3.1	Provincial Planning Statement.....	9
3.2	Regional Official Plan .....	9
3.3	City of Waterloo Official Plan (OPA 58) .....	9
4.	Conclusion.....	11

# 1. Introduction

The City of Waterloo (“City”) was awarded \$23.4 million in funding from the federal government’s Housing Accelerator Fund (HAF), a program that provides financial support to local governments to address housing affordability and supply challenges. The City has committed to nine initiatives as part of their HAF funding. One of these initiatives is a ‘Corridor Expansion Study and Plan’, which is intended to enable more “missing middle” transit supportive housing options in the Sugarbush South neighbourhood (“Sugarbush South”). In this context, “missing middle” refers to building typologies between three and eight storeys such as townhouses, stacked townhouses and mid-rise multi-unit apartment buildings. In other words, this means housing types that are bigger than single houses but smaller than high-rises.

The Sugarbush South neighbourhood is bounded by Columbia Street West to the south, Albert Street to the west, Smallwood Drive to the north, and High Street to the east. The neighbourhood is approximately 22 hectares in area and is located near two Major Transit Station Areas, three post-secondary institutions, the Northdale neighbourhood, the King Street North Major Corridor, and other services and amenities. The neighbourhood is a typical mid-20<sup>th</sup> century housing development containing primarily low rise residential dwellings. Sugarbush South was initially developed as a suburban community from the 1950s to the 1960s. With the continued growth of the post-secondary institutions, there has been significant pressure on the city’s near-campus neighbourhoods to accommodate students, staff and faculty. This has resulted in intensification in near-campus nodes and corridors, such as on the periphery of the neighbourhood (north side of Cardill Crescent and east of Holly Street), as well as the conversion of single detached dwellings into multi-unit dwellings. The neighbourhood has a varied demographic and plays an important role in providing rental housing.

In the City’s Official Plan, the Sugarbush South neighbourhood is primarily designated Low Rise Residential with some lands along Austin Drive, High Street, Cardill Crescent, and Hazel Street designed Medium Rise Residential. A small portion of the neighbourhood – being the north edge of Cardill Crescent – is located within the Research and Technology Major Transit Station Area (MTSA). Lands along Columbia Street West were redesignated as a Minor Corridor and Medium Rise Residential through the first phase of the City’s Official Plan Review (“OPA 58”), which was adopted by City Council in December 2024. OPA 58 is currently with the Province for final approval.

The Corridor Expansion Study and Plan for Sugarbush South is intended to update the land use for the neighbourhood to permit medium rise residential development – with limited mixed use permissions – at heights of up to six to eight storeys and densities of up to 450 bedrooms per hectare (and up to 600 bedrooms per hectare with a Zoning By-law Amendment). All lands in the neighbourhood are proposed to be redesignated to Medium Rise Residential in the City's Official Plan and zoned accordingly in the City's Zoning By-law 2018-050. Urban design guidelines have also been prepared to guide future medium rise redevelopment in the neighbourhood.

## 2. Sugarbush South Land Use Plan

This section provides an overview of the key directions of the Sugarbush South Land Use Plan and explains the purpose of the individual components of the Sugarbush Land Use Plan – the Official Plan Amendment, Zoning By-law Amendment, and Sugarbush South Urban Design Guidelines.

### 2.1 Overview of the Sugarbush Land Use Plan

This subsection summarizes the key objectives and directions of the Sugarbush Land Use Plan, which are implemented by the individual components of the plan.

#### 2.1.1 Enabling Medium Rise Residential & Missing Middle Housing

As part of the Sugarbush South Land Use Plan, all residential lands in the neighbourhood are redesignated to Medium Rise Residential in the City's Official Plan. The City's Official Plan states that the aim of this designation is to encourage *“medium density, mixed use, primarily residential development within Minor Corridors, recognizing that properties may also function as a transition from low rise areas to medium-high and high-rise areas”*. The primary objective of permitting Medium Rise Residential permissions in the Sugarbush South neighbourhood is to expand the range of housing options, particularly missing middle housing such as townhouses and up to six to eight-storey multi-unit residential buildings.

The Sugarbush South Land Use Plan enables missing middle housing broadly across the neighbourhood, while focusing increased height permissions in certain areas, notably along Columbia Street West and Albert Street that are identified as Minor Corridors and portions of Cardill Crescent which are within a Major Transit Station Area. Minor Corridors and Major Transit Station Areas are identified areas of focused growth in Provincial, Regional, and City policy. The following subsections outline the approach in further detail.

#### 2.1.2 Expanding the Minor Corridor to Albert Street

The City's Official Plan currently identifies a Minor Corridor along Columbia Street West. According to the City's Official Plan, Minor Corridors *“connect a series of Major Nodes, Minor Nodes and/or the Primary Node”* and *“planned land uses within Minor Corridors will be predominantly medium to medium high rise residential or employment”*. The Sugarbush South Land Use Plan extends the Minor Corridor along Albert Street between Columbia Street West and Cardill Crescent.

Expanding the Minor Corridor to Albert Street within Sugarbush South and enabling additional medium-rise residential uses along the expanded corridor will enable additional housing at a location in proximity to two Major Transit Station Areas and located along a higher order road (Major Collector) well-suited for intensification. Mixed uses are planned to be enabled along Albert Street within the Minor Corridor, which will support the achievement of a complete community. The extension would round out the existing Minor Corridor resulting in a continuous Minor Corridor that integrates with the existing Minor Corridor on Columbia Street West and provides another linkage between the University of Waterloo Station Area immediately to the south and the Research and Technology Station Area to the north.

### **2.1.3 Maximum Building Heights of 8 Storeys – Minor Corridors**

Within the Sugarbush South Land Use Plan, lands within the Minor Corridors (Columbia Street West and Albert Street) and Major Transit Station Area (north edge of Cardill Crescent) will be permitted to have a building height of up to eight-storeys and a maximum density of 450 bedrooms per hectare. The density can be increased up to 600 bedrooms per hectare with “bonusing” that may apply through a Zoning By-law Amendment. Heights of up to eight-storeys within these strategic growth areas align with a comparable existing or planned scale of development in the Northdale neighbourhood to the south and further enables a land use transition to lower planned heights in the interior of Sugarbush South.

### **2.1.4 Maximum Building Heights of Six (6) Storeys – Interior of Neighbourhood**

Lands located in the interior of the Sugarbush South neighbourhood are generally zoned to allow building heights of up to six-storeys and a maximum density of 450 bedrooms. The density can be increased up to 600 bedrooms per hectare with “bonusing” that may apply through a Zoning By-law Amendment. These areas include portions of Cardill Crescent, Hazel Street, High Street, Austin Drive, Tamarack Drive, Sorrel Place, and Smallwood Drive. Examples of permitted built forms within these areas would include multi-unit residential buildings up to six-storeys in height, townhouses, and stacked townhouses. Limited non-residential uses would be permitted.

### **2.1.5 Protecting Rental Housing – Existing Low to Medium Rise Apartments**

The Sugarbush South Land Use Plan enables building heights of up to eight storeys on lands that contain existing low to medium rise apartments. This area is generally bounded by Columbia Street West, Holly Street, Austin Drive, and High Street. These properties generally have larger lot sizes and are designated as Medium Rise in the Official Plan. Combined with the City’s goal of encouraging more missing middle housing in the area, these lands are well-suited for possible future redevelopment at heights above the standard six-storey limit of the Medium Rise designation. However, it is acknowledged that these

lands contain existing buildings with a high concentration of existing rental housing units. In an effort to protect the existing rental housing supply in this area, building heights on these lands will be limited to six-storeys until such time as the City passes a rental replacement by-law. The implementing Zoning By-law will include a holding provision related to these existing rental housing units.

#### **2.1.6 Street Frontages – Enabling Mixed Uses**

The draft zoning by-law permits residential and non-residential uses in the RS-6 and R-8 zone categories. The scale and intensity of non-residential uses varies between the zone categories. However, non-residential uses are not permitted on Smallwood Drive, Sorrel Place and Tamarack Drive. To provide for the potential mix of uses within the neighbourhood, the plan assigns one of two “street frontages” to streets within the neighbourhood – either “Convertible Frontage” or “Neighbourhood Frontage”.

Lands along the Minor Corridors on Columbia Street West and Albert Street are classified as Convertible Frontage, which will ensure that the ground floor of buildings are designed to accommodate non-residential uses at such time as these uses may be supported.

Lands in the interior of the neighbourhood are classified as Neighbourhood Frontage. These lands will also continue to permit a mix of residential and non-residential uses but development will not be required to be designed to accommodate mixed uses. Other policies apply to lands classified as Neighbourhood Frontage, including that ground floor residential uses must have unit entrances that connect to the street which helps activate the street.

The Sugarbush South Land Use Plan recognizes that not all areas within the neighbourhood are well-suited for non-residential uses; lots on Smallwood Drive, Sorrel Place, and Tamarack Drive do not include permissions for non-residential uses.

#### **2.1.7 Identifying Potential Pedestrian Corridors (Walkways)**

The implementing Official Plan Amendment for the Sugarbush South Land Use Plan includes a map which identifies potential pedestrian walkways in Sugarbush South. Three potential pedestrian walkways are identified as follows:

- A walkway between the south side of Cardill Crescent and Columbia Street West. This would provide a more direct pedestrian connection to Columbia Street West.

- A walkway between the north side of Cardill Crescent and lands further north which front Albert Street. This would provide a more direct connection to nearby lands to the north, which are anticipated to develop as they are within a Major Transit Station Area.
- A walkway between Sorrel Place and the intersection of Hazel Street and Austin Drive. This would improve pedestrian connectivity by providing a direct route from the cul-de-sac at Sorrel Place to the rest of the neighbourhood.
- A mid-block walkway between Austin Drive and High Street and High Street and Columbia Street West, west of Holly Street. This would improve pedestrian connectivity by providing a more direct route from Austin Drive to Columbia Street West.

Identification of potential walkways in this Plan will enable the City to secure these potential pedestrian walkways as the neighbourhood redevelops. The implementing Zoning By-law Amendment also includes holding provisions for the walkways, which would provide the City with an additional ability to secure the connections through the development process. These walkway connections in Sugarbush South are intended to improve pedestrian and community connectivity for current and future residents and visitors to the neighbourhood.

#### **2.1.8 Identifying Preferred Park/Parkette Locations**

The Sugarbush South Land Use Plan identifies potential parkette areas in the Urban Design Guidelines and identifies Potential Pedestrian walkways in the Official Plan Amendment with implementing policies in the Zoning By-law Amendment. If and when secured, the potential park/parkette locations are intended to meet the needs of persons of all ages and abilities and foster social interaction within Sugarbush South.

## **2.2 Components of the Sugarbush Land Use Plan**

The Sugarbush South Land Use Plan includes three key items to implement the plan – an Official Plan Amendment, Zoning By-law Amendment, and Sugarbush South Urban Design Guidelines. These are each outlined below.

### **2.2.1 Official Plan Amendment**

The Official Plan Amendment updates the City's Official Plan to reflect the changes for the Sugarbush South neighbourhood. The Official Plan Amendment includes the following:

- New Area Specific Policy 89 which provides policies for the Sugarbush South neighbourhood including vision and guidance to implement the Medium Rise Residential designation permitting heights of up to six to eight-storeys across the neighbourhood. Area Specific Policy 89 also provides specific “bonusing” like criteria for zoning by-law amendments which seek to increase the maximum density up to 600 bedrooms per hectare.
- New Area Specific Policy 89A which provides policies for the existing low to medium rise apartments area generally bounded by Columbia Street West, Holly Street, Austin Drive, and High Street. The policies provide direction to allow building heights of up to eight storeys in this area at such time as the City passes a rental replacement by-law.
- Mapping changes to designate the Sugarbush South neighbourhood as Medium Rise Residential, extend the Minor Corridor to Albert Street, identify the potential pedestrian corridors (walkways), and show the street frontage areas and street elements.

### **2.2.2 Zoning By-law Amendment**

The Zoning By-law Amendment updates the City’s Zoning By-law 2018-050 to implement the Official Plan Amendment. The Zoning By-law Amendment includes:

- Creation of two new zone categories – the Residential Sugarbush Six (RS-6) zone and the Residential Sugarbush Eight (RS-8) zone – which will apply to lands zoned for heights of six and eight-storeys, respectively, and permit maximum densities of up to 450 bedrooms per hectare. The new zones include a range of permitted residential and non-residential uses and additional regulations related to setbacks, street frontages, and parking, among others.
- Holding provisions for the RS-6 and RS-8 zones to ensure certain elements are considered and addressed prior to development including matters related to infrastructure, land use compatibility, potentially contaminated sites, adherence to urban design standards, securing mid-block walkway connections, and setbacks to overhead infrastructure (i.e. hydro lines).
- A Site-Specific Exception for the areas generally bounded by Columbia Street West, Holly Street, Austin Drive, and High Street which contain existing low to medium rise apartments. The Exception applies a holding provision limiting building height to six-storeys until such time as the City passes a rental replacement by-law.



- Application of street frontage areas. Convertible Frontages primarily apply to the Minor Corridors along Columbia Street West and Albert Street West. Neighbourhood Frontages generally apply to the interior of the neighbourhood.
- Mapping and other minor regulation changes.

### **2.2.3 Sugarbush South Urban Design Guidelines**

The Sugarbush South Land Use Plan includes the Sugarbush South Urban Design Guidelines. The guidelines conform to and complement the Official Plan's Urban Design policies while building upon the guidelines contained in the City's Urban Design Manual. The Sugarbush South Urban Design Guidelines provide a set of objectives customized to the Sugarbush South neighbourhood which recognize the unique characteristics of the area, including tree canopy coverage and overhead hydro infrastructure. The Urban Design Guidelines also include, for example, specific guidance for the types of lot consolidation that is anticipated to occur as the neighbourhood redevelops.

## 3. Policy Summary

This section provides a brief summary of how the Sugarbush South Land Use Plan considers the applicable Provincial, Regional, and local land use planning policy framework.

### 3.1 Provincial Planning Statement

The Provincial Planning Statement (“PPS”) came into effect on October 20, 2024 and is issued under section 3 of the *Planning Act* and requires that all planning matters be consistent with the PPS. The PPS contains a range of policies to implement the Province’s vision for land use and contains direction for housing, infrastructure, natural resources, and public health and safety, among others. The Sugarbush South Land Use Plan considers the Provincial Planning Statement by promoting a range and mix of housing through medium rise, transit supportive intensification within the settlement area and strategic growth areas, being the Minor Corridors and within the Major Transit Station Area. It locates higher densities along strategic growth areas, supports active transportation, enables mixed uses, and identifies opportunities for new parks, trails, and connections to support the creation of complete communities.

### 3.2 Regional Official Plan

The Regional Official Plan (“ROP”) was adopted by the Region of Waterloo in June 2009 and received final approval from the Ontario Municipal Board on June 18, 2016. On August 18, 2022, ROPA 6 was adopted, which accommodates Waterloo Region’s forecasted population growth to 2051. The Sugarbush South Land Use Plan takes guidance from the Regional Official Plan by directing growth to the Urban Area and strategic growth areas, supporting the Region’s intensification targets and objectives for complete, transit supportive neighbourhoods. It enables a more compact built form with a diverse range of housing options, including missing middle, while including measures to help protect existing rental units. The Plan also advances walking and cycling objectives by identifying potential new connections and requiring bicycle parking.

### 3.3 City of Waterloo Official Plan (OPA 58)

The City’s current Official Plan was originally approved in 2012. On December 2, 2024, Waterloo City Council adopted Official Plan Amendment 58 as part of Phase 1 of the City’s Official Plan Review. While OPA 58 was adopted by City Council, the Province is the final approval authority and has not yet approved the amendment. This Brief analyzes the City’s Official Plan as if OPA 58 is in full force and effect.

The Sugarbush South Land Use Plan aligns with the City of Waterloo's Official Plan by focusing intensification in strategic areas such as Minor Corridors and the Major Transit Station Area, while introducing missing middle housing in the interior of the neighbourhood. It applies the Medium Rise Residential designation broadly, permitting up to six-storeys generally, and up to eight-storeys in strategic growth areas and select areas with existing low to medium rise apartments (with protections for rental housing). The plan expands the Minor Corridor to Albert Street, supports mixed uses, improves parks and connectivity, incorporates urban design guidelines, and incorporate holding provisions to address infrastructure, compatibility, and community linkages, ultimately supporting complete communities.

## 4. Conclusion

As part of the City's HAF initiative for a 'Corridor Expansion Study and Plan', aimed at enabling "missing middle" housing options in the Sugarbush South neighbourhood, the City retained GSP Group to prepare the implementing Sugarbush South Land Use Plan.

The Sugarbush South Land Use Plan redesignates all residential lands in Sugarbush South to Medium Rise Residential to expand housing options – particularly missing middle housing forms such as townhouses, stacked townhouses and six to eight-storey multi-unit residential buildings. In the plan, maximum height permissions are focused along the Minor Corridors on Columbia Street West and Albert Street, and the Major Transit Station Area which applies to parts of Cardill Crescent. The plan also extends the Minor Corridor to Albert Street to strengthen connections between nearby Major Transit Station Areas, support mixed uses along key corridors, and enable limited mixed uses in interior areas. The plan includes measures to protect existing rental housing, identifies potential pedestrian corridors (walkways) to improve neighbourhood connectivity, and designates preferred park/parkette locations to enhance community spaces. The Sugarbush South Land Use Plan is implemented by an Official Plan Amendment, Zoning By-law Amendment, and Sugarbush South Urban Design Guidelines.

The Sugarbush South Land Use Plan considers direction from Provincial, Regional, and City policies by directing growth to strategic areas, promoting medium rise, transit supportive intensification, and encouraging a diverse mix of housing including missing middle forms while including measures to protect existing rental buildings in areas where they are more concentrated. It supports active transportation, mixed uses, and new parks and connections to support complete communities.

### **11.1.89 Area Specific Policy 89 (Sugarbush South Neighbourhood)**

- (1) The policies of this Area Specific Policy (ASP) apply to lands known as the Sugarbush South Neighbourhood, bounded by Columbia Street West, Albert Street, Cardill Crescent, Smallwood Drive, and High Street, shown as ASP 89.

#### **Vision**

- (2) Sugarbush South is a neighbourhood in transition. It is anticipated that Sugarbush South will accommodate a portion of the City's medium rise intensification growth over the planning period of this Official Plan given the proximity of the neighbourhood to the post-secondary institutions, designated Nodes and Corridors and two Major Transit Station Areas (MTSAs). As such, the policies of this Plan support the redevelopment of Sugarbush South as a vibrant, sustainable and *mixed-use* neighbourhood. Sugarbush South will become a complete neighbourhood that accommodates a range of housing types and commercial uses in the Minor Corridors, and which supports *active transportation* and increased medium rise densities.

#### **Objectives**

- (i) Development in Sugarbush South will achieve the following objectives:
- (ii) Development will be appropriately scaled, massed, and oriented to respect adjacent properties, existing vegetation, and the overall medium rise planned function of the neighbourhood.
- (iii) A network of walkable streets, mid-block connections, and pedestrian-oriented frontages will create a cohesive, human-scaled neighbourhood that supports active transportation and transit use.
- (iv) The mature tree canopy will be protected wherever possible with new development incorporating tree retention, front yard planting, and green buffers consistent with the City's Comprehensive Engineering and Landscape Manual.
- (v) A range of unit types, sizes and tenures including family-friendly formats will support a mix of household needs and life stages.
- (vi) Lot consolidation will be encouraged to enable viable redevelopment and coordinated site design.

- (vii) Built form transitions between new mid-rise development and existing low-rise dwellings will be managed through setbacks, height limits, and stepbacks to reduce perceived scale.
- (viii) Development should incorporate low-impact design practices such as green infrastructure, energy-efficient buildings, and compact land use patterns. Coordination with hydro infrastructure and stormwater systems will ensure long-term resilience.

### **Preferred Neighbourhood Elements**

- (3) A number of preferred neighbourhood elements have been identified which provide more detailed planning and land use objectives to guide redevelopment in Sugarbush South. The preferred neighbourhood elements below provide the basis for the land use plan for Sugarbush South, which is implemented through the land use designations and policies of this Plan, the implementing Zoning By-law, the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines. The following preferred neighbourhood elements provide guidance to the redevelopment of Sugarbush South.

### **Neighbourhood Structure:**

- NS.1 The City will favour the transition from a primarily low-density neighbourhood to a medium rise residential community with an emphasis on enabling medium rise housing forms such as stacked townhouses and medium rise apartments.
- NS.2 New development will be focused on mid-rise multiple residential development, transit and *pedestrian* supportive principles with mixed uses directed to Minor Corridors, allowing for a diversity of people and activities.
- NS.3 New development will consider the provision of affordable and/or attainable housing options where appropriate.
- NS.4 Clear land use, urban design and built form policies, regulations and guidelines will guide future *development*. Building height and density will generally be slightly greater on the periphery of the neighbourhood, and transition towards to mid-rise building forms at lower heights in the interior of the neighbourhood.

### **Buildings & Development:**

- BD.1 New buildings, enhancements to the public realm and related development elements should be constructed in a sustainable manner, conserving energy and

resources, using durable materials and where appropriate, consider principles and strategies of net-zero design.

- BD.2 Future development will be encouraged to maintain consistency with the existing street edge by establishing complementary front yard and street line setbacks and urban design/built form policies and guidelines to create a sense of an active and vibrant streetscape, limit breaks in the active street facade, and maximize the principles of “eyes on the street”.
- BD.3 Appropriate and compatible building design techniques will be used, including transition in building heights, step-backs, and building articulation.
- BD.4 The location of ground-floor windows, doors, and main entranceways will support visibility and transparency at grade, and access from the street.
- BD.5 Indoor and outdoor common amenity areas will be required in accordance with the implementing Zoning By-law for all housing developments to promote a healthy social environment both within buildings and along the street. Outdoor elements could include porches and patios adjacent to the street, private balconies with appropriate privacy design considerations and small common courtyard and/or amenity areas that provide for connectivity to the public realm. Indoor common amenity areas may include recreational, study and living areas.
- BD.6 Parking areas will not be permitted in the front yard, and rear yard parking will be preferred. Structured or structured parking will be encouraged for higher density building forms on consolidated lots where appropriate.

#### **Public Realm:**

- PR.1 The Sugarbush South neighbourhood includes convertible street frontage areas identified along Columbia Street West and Albert Street. Convertible street frontage areas will be designed to be convertible from residential to an animated retail, commercial or *community* use at a time when it can be supported.
- PR.1 The neighbourhood contains an existing mature tree canopy, typical in low rise residential neighbourhoods, and consists primarily of street trees in the right-of-way and privately owned trees in the yards of properties. A part of new development, every effort will be made to preserve the existing tree canopy to the extent possible or to maximize opportunities for new trees or other landscaping measures. This may include the strategic location of access driveways (including utilizing existing accesses where possible), exploring opportunities for additional tree plantings on City land, and maximizing landscaping opportunities through the *Site Plan* Control process

- PR.2 The neighbourhood contains existing overhead power lines and related infrastructure in the front and rear yards of some existing lots. Where permitted by the regulatory authority, underground relocation of this infrastructure will be encouraged through redevelopment. Where overhead power lines are maintained through redevelopment, buildings and structures will be required to adhere to any required setbacks to this infrastructure, as determined by the regulatory authority. The implementing Zoning By-law may address compliance with building setbacks to infrastructure as a consideration for *development* applications
- PR.3 Opportunities to enhance and create small public and private parks, parkettes, walkways and other active open spaces, will be explored to foster connectivity and expand convenient access to publicly-accessible amenity spaces. These opportunities could include the acquisition of park space and trails by the City as well as opportunities for public access on privately-owned parks and amenity spaces.
- PR.4 Improvements to *pedestrian*, cycling and rolling connections will be established through the redevelopment of lands to promote connectivity and provide better access.
- PR.5 The development of new buildings and the public realm will incorporate principles of Crime Prevention Through Environmental Design (CPTED). New development will also have regard for the principles of universal accessibility.
- PR.6 Improved streetscaping should be provided and may include tree lined boulevards, enhanced landscaping, paving and traffic calming, and where feasible and appropriate, through the provision of attractive street furniture and hardscape to define the public realm and neighbourhood character.

### **Neighbourhood Structure**

- (4) The neighbourhood structure for Sugarbush South builds upon the City of Waterloo's planned city form and growth management objectives outlined in Section 3.1 of the Official Plan and should be read in conjunction with these policies. The neighbourhood structure for Sugarbush South is consistent with the City's designated Nodes and Corridors planning framework by focusing more intensive development in Minor Corridors along Columbia Street West and Albert Street and will support the Region and City's planning initiatives for more intensive development within proximity to Major Transit Station Areas.



## **Designated Nodes and Corridors**

- (5) Section 3.5 of the Official Plan establishes a hierarchy of Nodes and Corridors which are identified on **Schedule 'B' - City Structure** and are intended to accommodate a significant portion of the City's population and employment growth through intensification and redevelopment. The Minor Corridor along Columbia Street West and Albert Street as identified on **Schedule 'B' - City Structure** provides sufficient lot depth and redevelopment opportunity, to accommodate additional corridor intensification.

The policies of Section 3.5 of this Plan shall continue to apply to the designated Corridors in the Sugarbush South neighbourhood, and in accordance with the policies of this Section.

## **Major Transit Station Areas**

- (6) A portion of the Sugarbush South neighbourhood is located within the Research and Technology Station Area, as shown on **Schedule 'J3' – Research and Technology Station Area**. Section 3.5.3 of this Plan outlines a policy framework for these areas, including direction related to land use, the public realm and place making and mobility. The policies of Section 3.5.3 of this Plan shall continue to apply to the portion of Sugarbush South located within the Research and Technology Station Area, and in accordance with the policies of this Section.

## **Permitted Height and Density**

- (7) The maximum permitted heights in Sugarbush South are identified on Schedule 'B1' – Height and Density, which guides built form and provides standardized permitted heights throughout the City. The categories of maximum permitted heights are outlined in Section 3.4 (1) of the Official Plan and apply to the Sugarbush South neighbourhood and shall be read in conjunction with the associated land use designation and applicable policies of this Plan.

## **Land Use**

### **Affordable Housing**

- (8) In accordance with the policies of Section 10.1.2 (9) to (14) of the Official Plan, new development in the Sugarbush South neighbourhood should provide for a range and mix of housing options with an overall target of a minimum of 30% of new home ownership and rental housing being affordable.

## Land Use Designations

- (9) The land use designations for Sugarbush South are identified on **Schedule 'A' - Land Use Plan** of the Official Plan. In addition to the land use designation policies of Section 10.0 of the Official Plan, the following additional policies shall apply to lands within Sugarbush South.
- (10) Sugarbush South is planned to be an area that transitions from low rise residential forms to a medium rise residential neighbourhood with some mixed uses primarily along significant corridors. Lands in Sugarbush South are primarily designated Medium Rise Residential. This Area Specific Policy modifies the Medium Rise Residential land use policies of the Official Plan to enable and optimize opportunities for missing middle housing forms in Sugarbush South. Sugarbush South is well-suited for this transition due to its strategic location in proximity to two Major Transit Station Areas, three post-secondary institutions, location along a Minor Corridor (Columbia Street West and Albert Street), and proximity to many services and amenities. The Medium Rise Residential designation for Sugarbush South is also intended to facilitate a transition to lower rise forms located north of Sugarbush South.

## Medium Rise Residential

- (11) For lands designated Medium Rise Residential as shown on **Schedule 'A' - Land Use Plan**, the policies of Section 10.1.4 shall apply, in addition to the following policies:
- (a) The scale of uses within the Medium Rise Residential designation shall be limited as shown on **Schedule 'B1' – Built Form** and the following regulations apply:
- (i) For lands identified as being within a Minor Corridor on **Schedule 'B' – City Structure** and/or within a Station Area on **Schedule 'J1' – 'J5' – Station Areas**, the maximum height shall be 8 storeys.
- (b) Notwithstanding the policies of Section 10.1.4 (3), increases in the permitted maximum building height may be permitted by way of a Zoning By-law Amendment, provided:
- (i) The maximum net residential density permitted on any one site shall not exceed 600 *bedrooms* per hectare.
- (ii) The maximum height of any building shall generally not exceed 8 storeys within a Minor Corridor **Schedule 'B' – City Structure** and/or Station Area on **Schedule 'J1' – 'J5' – Station Areas**.

- (iii) Sufficient lot area and/or lot consolidation has been demonstrated.
  - (iv) The site does not require significant relief from standard regulations including parking, landscaping, amenity space and other site-specific requirements.
  - (v) Adequate active transportation connections, including pedestrian/cycling paths and walkways, have been provided in accordance with the policies of this Plan, the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines as applicable.
  - (vi) There is a mix of unit types and the inclusion of multi-*bedroom* units.
  - (vii) Opportunities for sufficient landscaping, including trees where feasible, will not be significantly impacted by the increase in net residential density.
  - (viii) The property shall be zoned a site-specific zone through the implementing Zoning By-law Amendment.
- (c) A mixture in the number of *bedrooms* within each dwelling unit shall be strongly encouraged to accommodate a diverse range and type of dwelling units.
- (d) In addition to the policies of Section 10.1.4 (5), lands designated Medium Rise Residential may be zoned to permit offices, *medical clinics*, *convenience retail*, restaurants, *food stores*, child care centres, and personal services as *ancillary uses*.
- (e) Where residential dwellings are proposed on the first storey and the lands are identified as a Neighbourhood Frontage Area on **Schedule 'A45a' – Street Frontage Areas and Streetscape Elements**, the residential dwellings shall accommodate primary entrances to each dwelling unit at grade, windows to provide street overlook, and be in accordance with the policies of Section 11.1.89 (13).
- (f) In review of development applications, the proposal shall comply with the following criteria to the satisfaction of the City:
- (i) The proposal will achieve a high standard of urban design, with emphasis on:
    - Site and building design that complement and enhance the surrounding neighbourhood character;

- Landscape design that creates visual interest; and,
  - Prominent entrances that emphasize the streetscape.
- (ii) The proposal shall comply with the urban design, land use and compatibility policies of this plan and shall demonstrate that the proposal implements the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines to the satisfaction of Council;
  - (iii) Sufficient *infrastructure* exists to service the site;
  - (iv) The development capacity of the site is not exceeded, and the proposal does not require significant relief from standard regulations;
  - (v) Minimum requirements for the provision of outdoor amenity space will be stipulated in the implementing Zoning By-law.
  - (vi) The proposal complies with other criteria that may be deemed appropriate by the City based on the context of the site.
- (g) The implementing Zoning By-law may establish minimum lot frontage and/or minimum lot area requirements, which encourages lot consolidation and the transition of the area to medium rise forms.

## Street Frontage Areas

- (12) **Schedule 'A45a' – Street Frontage Areas and Streetscape Elements** identifies street frontage areas within Sugarbush South, which provides additional guidance for the permitted land uses and built form requirements at the ground floor frontage, in accordance with the following policies:

### (a) Convertible Frontage Areas

- (i) The permitted uses within a Convertible Frontage Area as identified on **Schedule 'A45a' – Street Frontage Areas and Streetscape Elements**, shall be in accordance with the permitted uses of the applicable land use designation. Notwithstanding the permitted uses at the ground floor, the ground floor areas shall be planned and designed to accommodate mixed commercial, retail, institutional, and/or community uses at the ground floor, at such time as these uses may be supported.
- (ii) Convertible Frontage Areas shall be designed to be easily converted to retail/commercial uses and shall be designed as flexible spaces,

with taller ground floors to accommodate a range of active uses with larger window/display areas and entrances.

- (iii) Buildings associated with a Convertible Frontage Area shall be appropriately setback from the street to accommodate active ground floor uses which enhance the street. The implementing zoning by-law shall establish minimum and maximum building setbacks.
- (iv) A taller ground floor storey along Convertible Street Frontage Areas will be established in the implementing Zoning By-law, to accommodate non-residential uses.
- (v) The implementing Zoning By-law shall establish requirements for Convertible Frontage Areas.

**(b) Neighbourhood Frontage Areas**

- (i) The permitted uses within a Neighbourhood Frontage Area, as identified on **Schedule 'A45a' – Street Frontage Areas and Streetscape Elements**, shall be in accordance with the permitted uses of the applicable land use designation.
- (ii) Dwelling units which are located at grade within a Neighbourhood Frontage Area shall provide direct access to the street from the main dwelling unit entrance.
- (iii) Buildings associated with a Neighbourhood Frontage Area shall be appropriately setback from the street and provide sufficient space for entrances and on-site landscaping and provide windows to provide street overlook. The implementing zoning by-law shall establish minimum and maximum building setbacks.
- (iv) The implementing Zoning By-law shall establish requirements for Neighbourhood Frontage Areas.

**Urban Design Guidelines**

- (13) This Plan provides policies which encourage a high standard of urban design for the public and private sector projects that contribute towards a memorable, attractive and livable City, designed for people and with a strong sense of place. The general urban design policies of Section 3.9 provide design objectives applicable to the Sugarbush South neighbourhood as well as policies specific to the Nodes and Corridors.

The Urban Design policies of this Plan are further supported by the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines, which provides specific built form, landscaping and sustainable site and building design and related site planning guidelines to assist Council in achieving the vision and principles established for the Sugarbush South neighbourhood. The Sugarbush South Urban Design Guidelines shall implement the Official Plan policies, Zoning By-law regulations and form a basis for the development approvals process and future public realm improvement considerations.

## **Networks, Trails and Open Space**

### **Transportation**

- (14) Section 6.0 of the Official Plan provides policies that support the integration of transportation and land use decisions which support compact, *mixed-use* urban forms, and safe and efficient multi-modal transportation systems, and these policies shall continue to apply to Sugarbush South.
- (15) A Transportation Impact Study may be required in support of a *development* application, and where a Zoning By-law amendment is required to provide additional height and/or density. The implementing Zoning By-law may zone lands with a holding provision ("H" symbol) to require the preparation of a Transportation Impact Study to assess the transportation impacts of the proposed *development*.

### **Road Network**

- (16) Section 5.4 of the Official Plan establishes a hierarchy of road classifications as identified on **Schedule 'E' – Road Classification System**. **Schedule 'G' – Road Allowances** identifies planned road allowances in accordance with Section 5.4.7 of the Plan. The Plan provides for all *roads* to function as *complete streets* which are planned, designed, operated and maintained to enable users of all ages and abilities to interact and move safely. Furthermore, City streets are to be planned and designed to incorporate green development techniques, including stormwater treatment which uses natural processes and landscaping to create visually and environmentally enhanced *roads*.
- (17) *Development* shall provide for an attractive streetscape consistent with the policies of this Plan and the City's Urban Design Manual and Sugarbush South Urban Design Guidelines.
- (18) **Schedule 'F' – Active Transportation Framework**, identifies potential *pedestrian* corridors to facilitate the creation of a more connected, walkable neighbourhood, integrated with the adjacent neighbourhoods and streets. Through the

implementation policies of this Plan, the City will seek to acquire these potential *pedestrian* corridors through the *development* approvals process.

## **Parking**

- (19) Section 6.9 of the Official Plan contains policies to ensure safe and attractively designed vehicular parking areas or facilities which accommodate sufficient on and off-street parking to accommodate development.
- (20) Sugarbush South is envisaged as a transit-oriented and *active transportation* neighbourhood with some mixed uses. Alternative modes of transportation will be supported to minimize the need for vehicular travel and parking. The Zoning By-law shall establish appropriate parking standards and bicycle parking standards, in accordance with the policies of Section 6.6 of the Official Plan.

## **Trails and Active Transportation**

- (21) Section 5.3.3 of the Official Plan establishes policies for the provision of linear corridors that provide connectivity through trails and open spaces. **Schedule 'F' – Active Transportation Framework** identifies major high level trail connections throughout the City, and these policies shall apply to the Sugarbush South neighbourhood.
- (22) The active transportation network within Sugarbush South provides for the identification of Columbia Street West, Albert Street, Hazel Steet, High Street and Austin Street as City-Wide Cycling and Multi-Use Routes.

## **Sustainability**

- (23) The Official Plan provides policies to promote sustainability objectives related to environmental sustainability (Section 8.7), sustainable buildings (Section 8.7.3), air quality and climate change (Section 8.6), and energy (Section 8.5). Additional guidance with respect to sustainability strategies for Sugarbush South are included in the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines. All planning and land use decisions should be consistent with and implement these guidelines.

## **Public Realm**

- (24) The public realm, including neighbourhood streets, *pedestrian* walkways, open spaces and trails should promote compact, walkable, vibrant spaces, which provide benefits to the residents, and the City. The following policies shall influence land use decisions within the public realm:
  - (a) Priority shall be given to creating a walkable and connected neighbourhood

which encourages walking, cycling and active modes of transportation, and promotes *pedestrian* activity and vibrant streetscapes.

- (b) Smaller parkettes and plazas shall provide community amenity spaces and foster interaction and sense of community. Cohesive public and private spaces are encouraged throughout the community to foster interaction and a sense of place.
- (c) Tree plantings shall be incorporated into streetscaping to promote large street tree canopies and comfortable micro-climates.
- (d) Encouraging a mixture of residential housing options in terms of housing types, tenures and built forms.

### **Private Developments**

- (25) New development is encouraged to pursue green building certification or implement green building strategies. Green building strategies for private development consist of both the development and planning of individuals sites, and the construction or renovation of buildings.

### Site Development

- (a) Site planning should seek to maximize outdoor amenity areas and green space as well as incorporate soft-landscaping techniques and native and adaptive plants species were appropriate
- (b) Stormwater management shall be collected and managed on-site in accordance with City standards.
- (c) Outdoor lighting should minimum light trespass across property boundaries while providing sufficient, uniform and safe lighting to address security considerations.
- (d) Bicycle racks and associated facilities should be provided to encourage cycling and active transportation and promoting opportunities for care-sharing and carpooling, while minimizing parking requirements.

### Building Development

- (a) Indoor water use should be minimized through low-consumption fixtures and potential reuse of roof water and/or grey water.



- (b) Reduced energy intensive building systems should be encouraged through the use of building materials, orientation, lighting, HVAC systems and monitoring equipment.
- (c) The reuse of existing building materials is encouraged as well as the use of durable, long-last building materials.
- (d) Indoor environments should minimize pollutants and provide for improved air quality and ventilation through the use of appropriate finishes, interior access to daylight spaces and comfortable indoor spaces.
- (e) Building entrances should be easily accessible from the street and parking areas, and be clearly visible. Building operations and maintenance should ensure that buildings are operating in an efficient manner.

## **Implementation**

### **Holding Provisions**

- (26) The holding provisions of Section 12.2.5 of the Official Plan shall continue to apply with respect to the implementation of holding zones in Sugarbush South. Holding provisions (“H” symbol) will be used by the City in situations where it is necessary or desirable to zone lands for development, but where development must be delayed pending completion or realization of one or more of the matters identified in Section 12.2.5 of this Plan, in addition to the following:
  - (a) Demonstrating that the development is consistent with the urban design, land use and compatibility policies of this Plan and implements the City’s Urban Design Manual and the Sugarbush South Urban Design Guidelines to the satisfaction of Council, through *development* applications.
  - (b) Completion of a Transportation Impact Study, as required, to the satisfaction of the City of Waterloo and/or the Regional Municipality of Waterloo.
  - (c) Securing potential *pedestrian* corridors identified on **Schedule ‘F’ – Active Transportation Framework**, where applicable, to the satisfaction of the City of Waterloo.
  - (d) Satisfying all required clearances to infrastructure, including overhead powerlines, to the satisfaction of the City of Waterloo.
  - (e) The City passes a by-law under Section 99.1 of the Municipal Act, 2001 to prohibit and/or regulate the demolition and conversion of existing residential rental properties having six or more units in Area Specific Policy 89A.

## Implementation Tools

- (27) The policies of ASP 89 shall be implemented in accordance with the policies and tools established in Chapter 12 of this Plan, including, but not limited to: Zoning By-laws, holding provisions, *site plan* control, subdivision control, temporary use by-laws, property maintenance by-laws, development charges by-law, complete *development* applications and submission requirements.
- (28) *Development* applications, including applications for *site plan* approval shall be consistent with the urban design, land use and compatibility policies of this Plan and shall demonstrate that the development implements the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines to the satisfaction of Council.

### 11.1.89A Area Specific Policy 89A (Existing Medium Rise Uses)

- (1) The policies of this Area Specific Policy apply to lands generally bound by Columbia Street West, Holly Street, Austin Drive, and High Street, shown as ASP 89A.
- (2) The policies of this ASP provide more direction for these lands, building upon the policy direction for Sugarbush South in ASP89. This area contains existing medium rise residential buildings, generally between three and four storeys in height. These lots have historically contained the type of missing middle housing the City envisages occurring at a broader scale across the broader Sugarbush South neighbourhood. These lands generally have a larger lot area compared to single detached house lots elsewhere in Sugarbush South and have long been designated as medium density lands by this Plan. These factors, along with the City's objective of incentivizing additional missing middle housing forms in Sugarbush South, make these lands favourable to longer-term redevelopment at building heights higher than the base 6 storeys permitted by the base Medium Rise Residential designation. As such, the maximum height shall be 8 storeys on these lands.

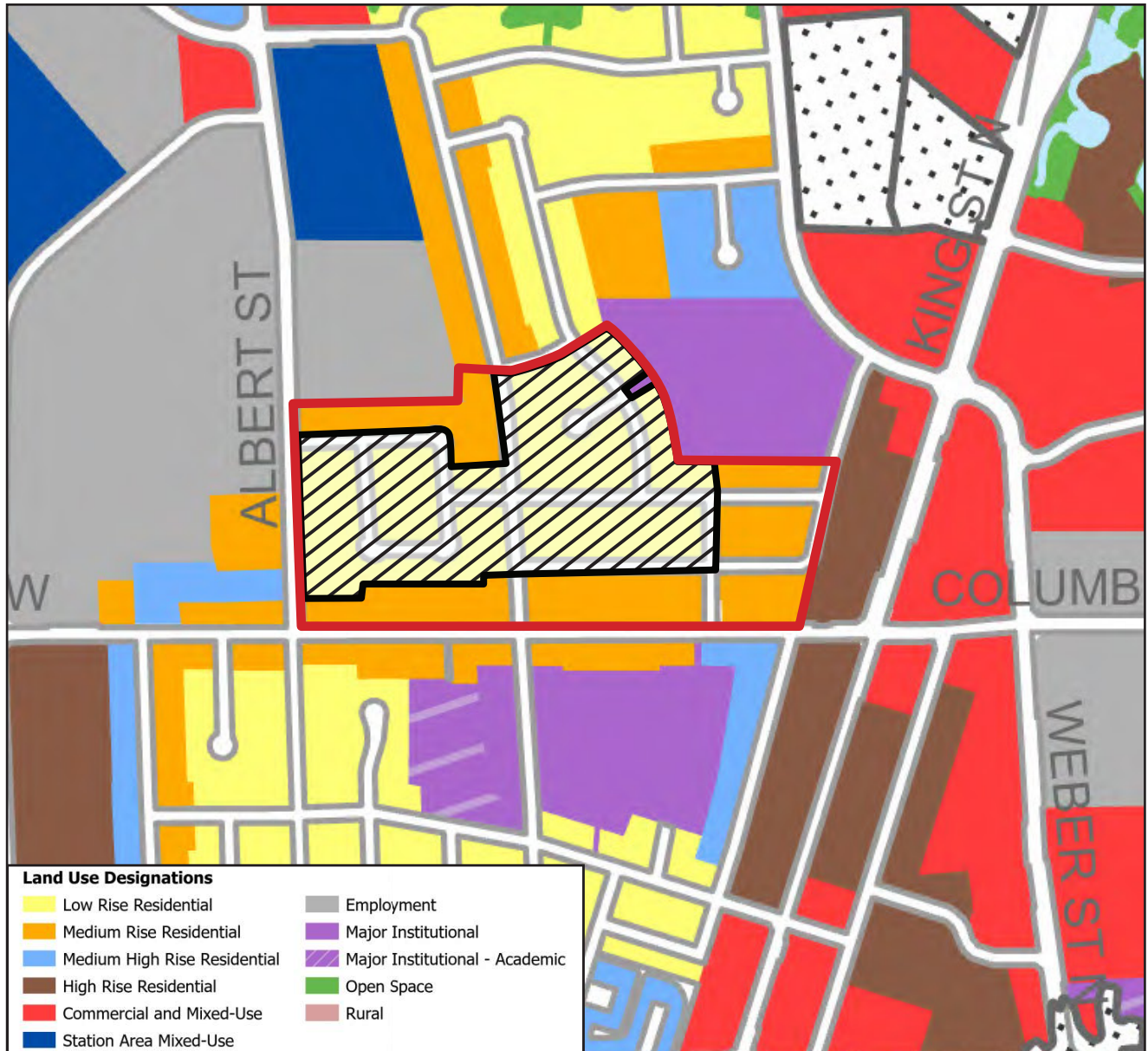
However, redevelopment of existing medium rise residential buildings at higher heights is foreseen to impact existing tenants of these buildings. Where an existing medium rise residential building is redeveloped, the City and *development* applications will consider impacts of redevelopment on tenants of the existing buildings planned to be redeveloped, while balancing the overall objective of incentivizing new missing middle housing in Sugarbush South.


- (3) Notwithstanding maximum height permissions in ASP 89 (8), as an interim condition to consider impacts of redevelopment of existing medium rise residential buildings within these lands, the implementing Zoning By-law will place a holding provision ("H" symbol) on the subject lands to apply a lower maximum height until such time as the City of Waterloo passes a by-law under Section 99.1 of the Municipal Act,

2001 to prohibit and/or regulate the demolition and conversion of residential rental properties to a purpose other than the purpose of a residential rental property. The intent of the holding provision ("H" symbol) is to maintain similar height permissions as have long been permitted by the Official Plan and the implementing Zoning By-law, while enabling additional height permissions when additional measures are in place to consider impacts of redevelopment on existing tenants.



MAP 1  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**SCHEDULE A - LAND USE**



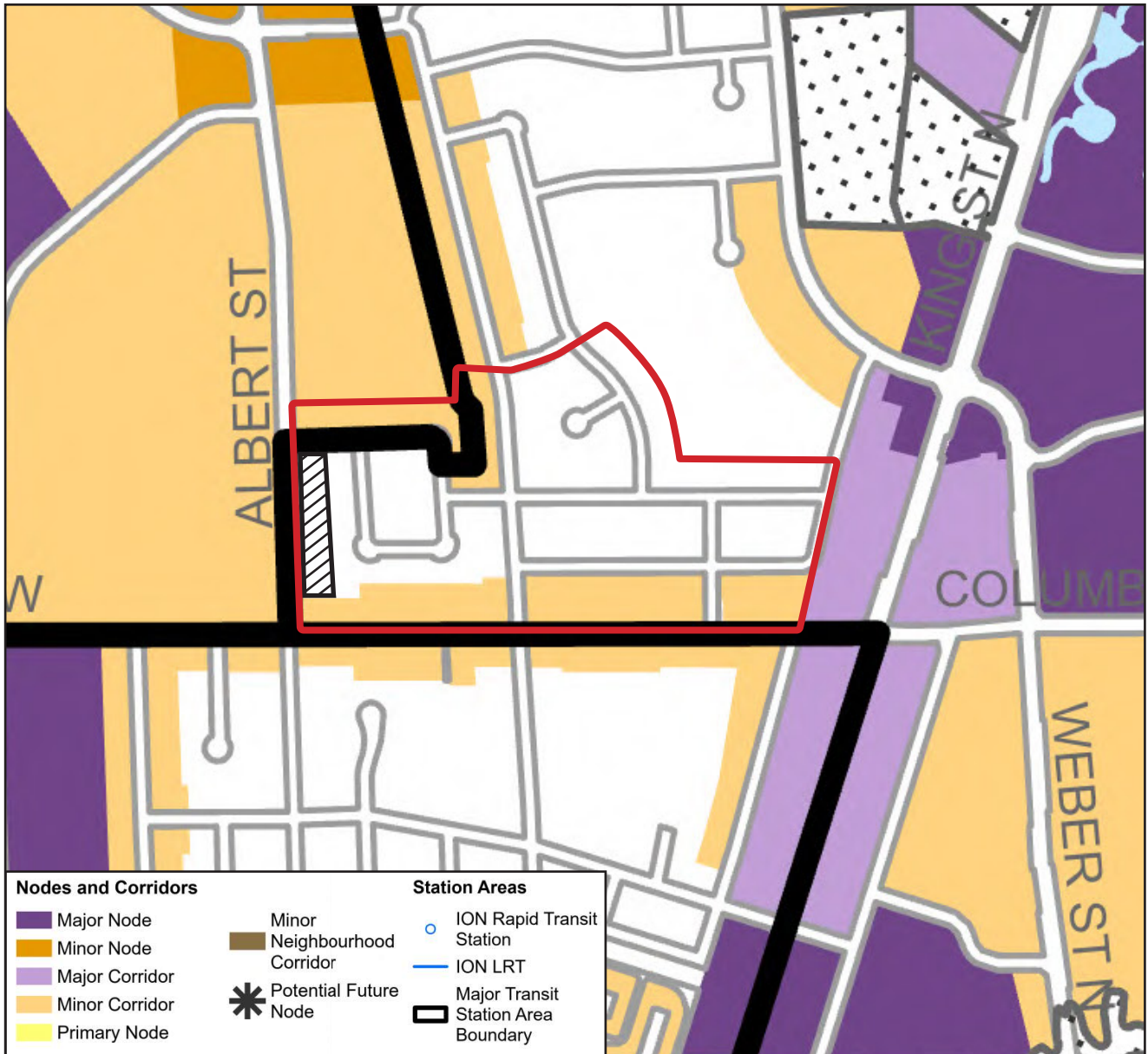
 SUGARBUSH SOUTH BOUNDARY

 AREA TO BE REDESIGNATED FROM LOW RISE RESIDENTIAL TO MEDIUM RISE RESIDENTIAL



N.T.S

MAP 2  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**SCHEDULE B - CITY STRUCTURE**



SUGARBUSH SOUTH BOUNDARY



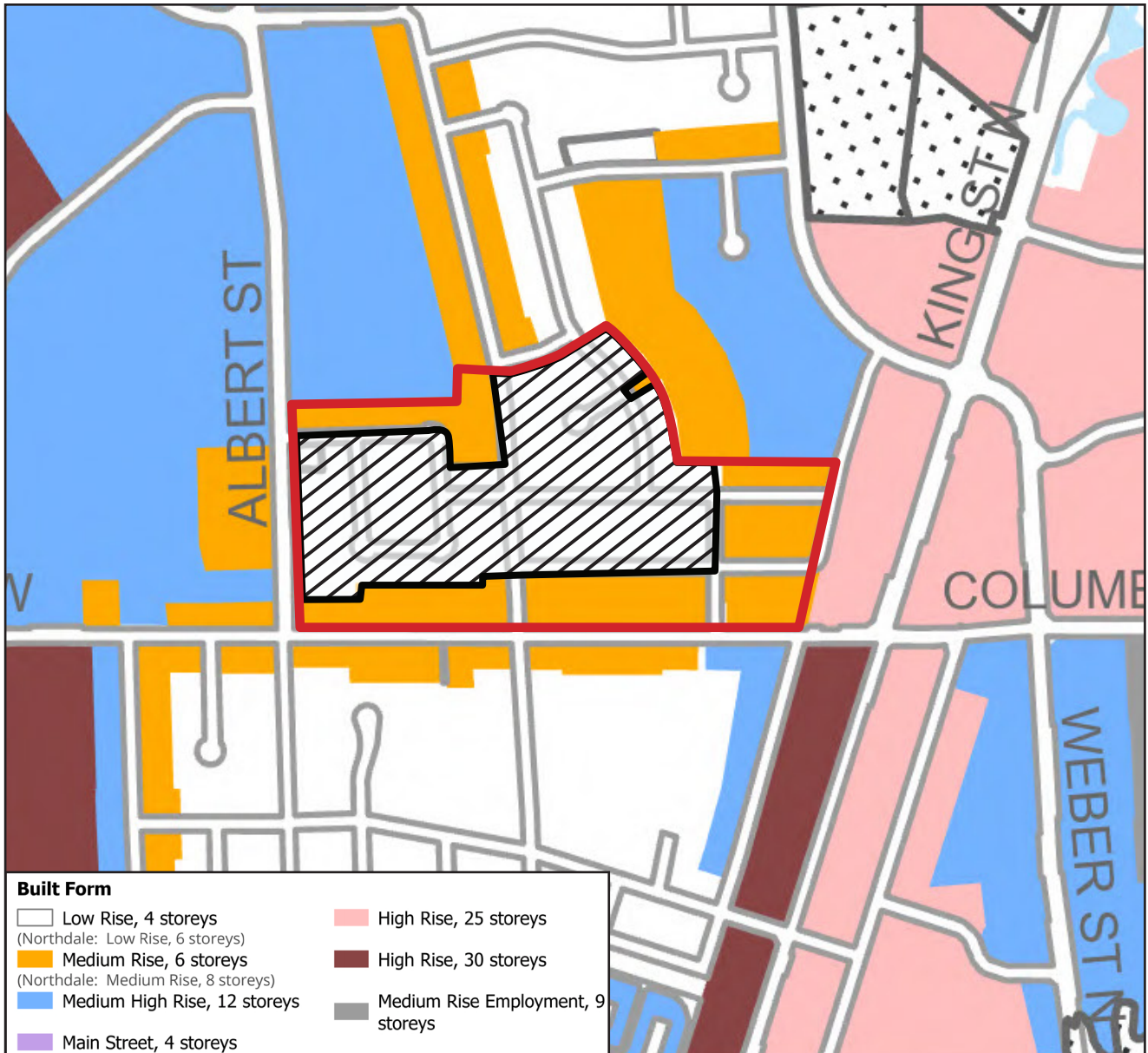
AREA TO BE DESIGNATED MINOR CORRIDOR




N.T.S



MAP 3  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**SCHEDULE B1 - BUILT FORM**



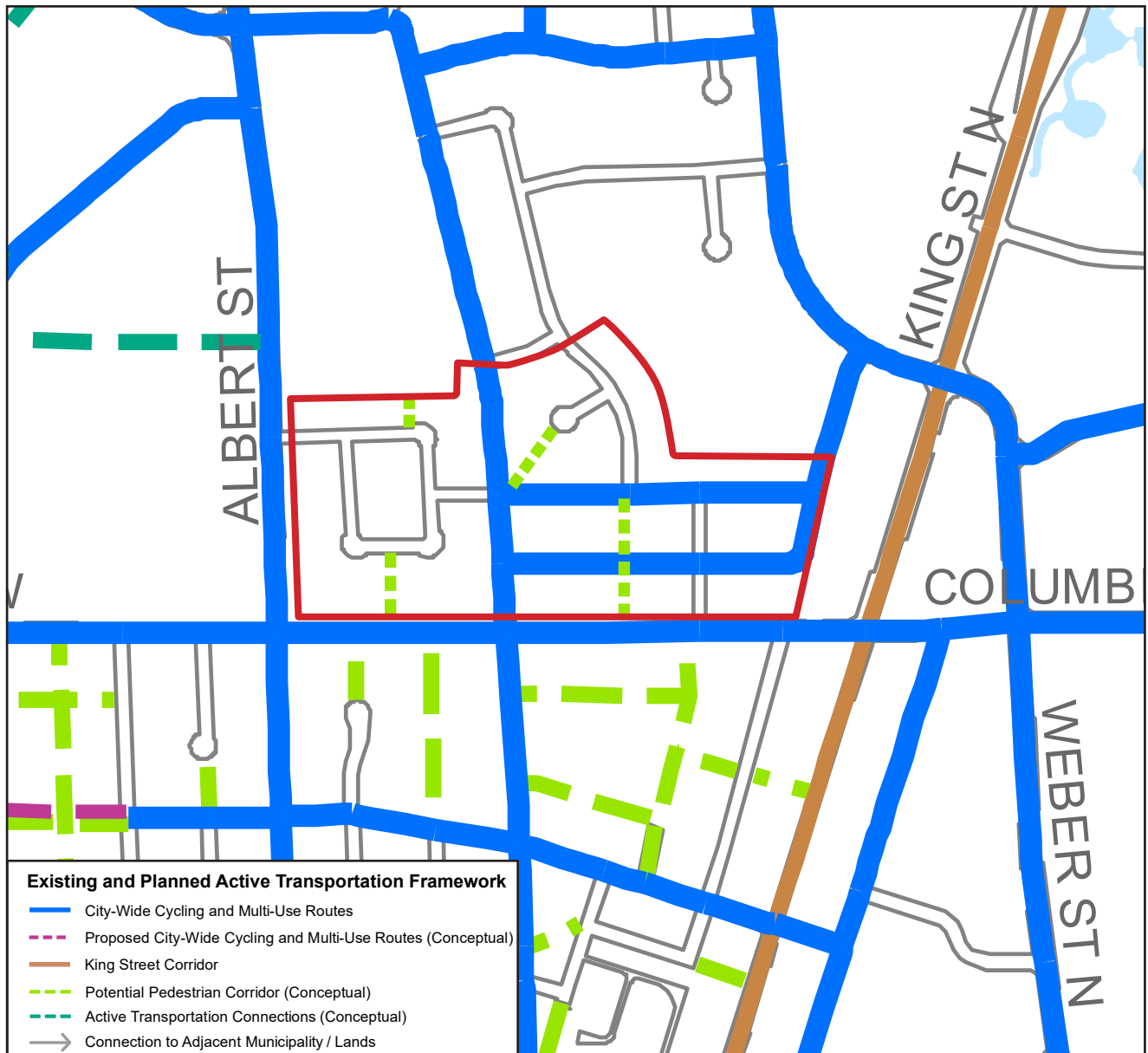
 SUGARBUSH SOUTH BOUNDARY

 AREA TO BE REDESIGNATED FROM LOW RISE TO MEDIUM RISE (6 STOREYS)



N.T.S

MAP 4  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**SCHEDULE F - ACTIVE TRANSPORTATION**



SUGARBUSH SOUTH BOUNDARY



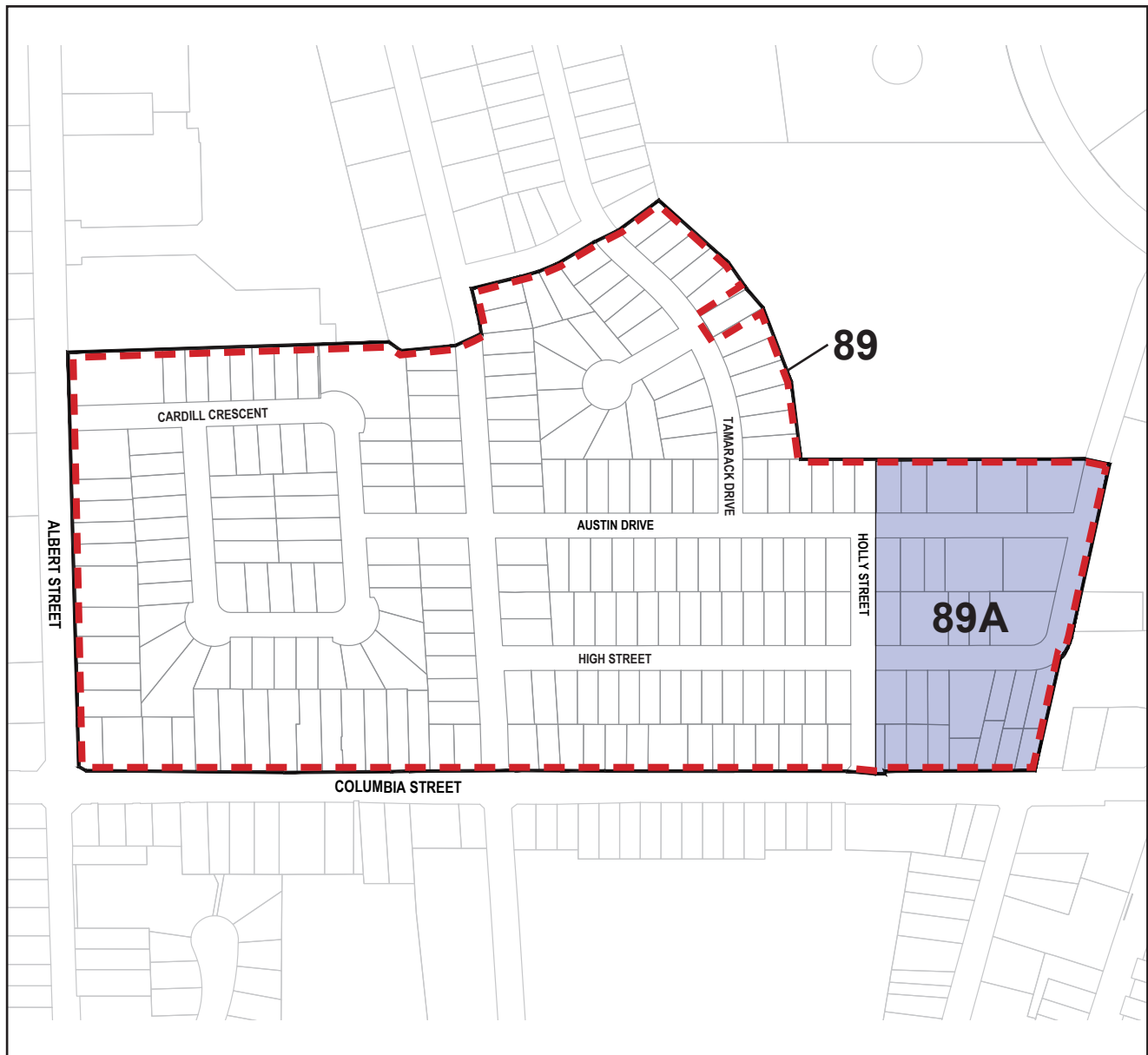
POTENTIAL PEDESTRIAN CORRIDOR TO BE ADDED



N.T.S



MAP 5  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**SCHEDULE A6a - AREA SPECIFIC POLICIES 20,45 & 89**



AREA SPECIFIC POLICY 89

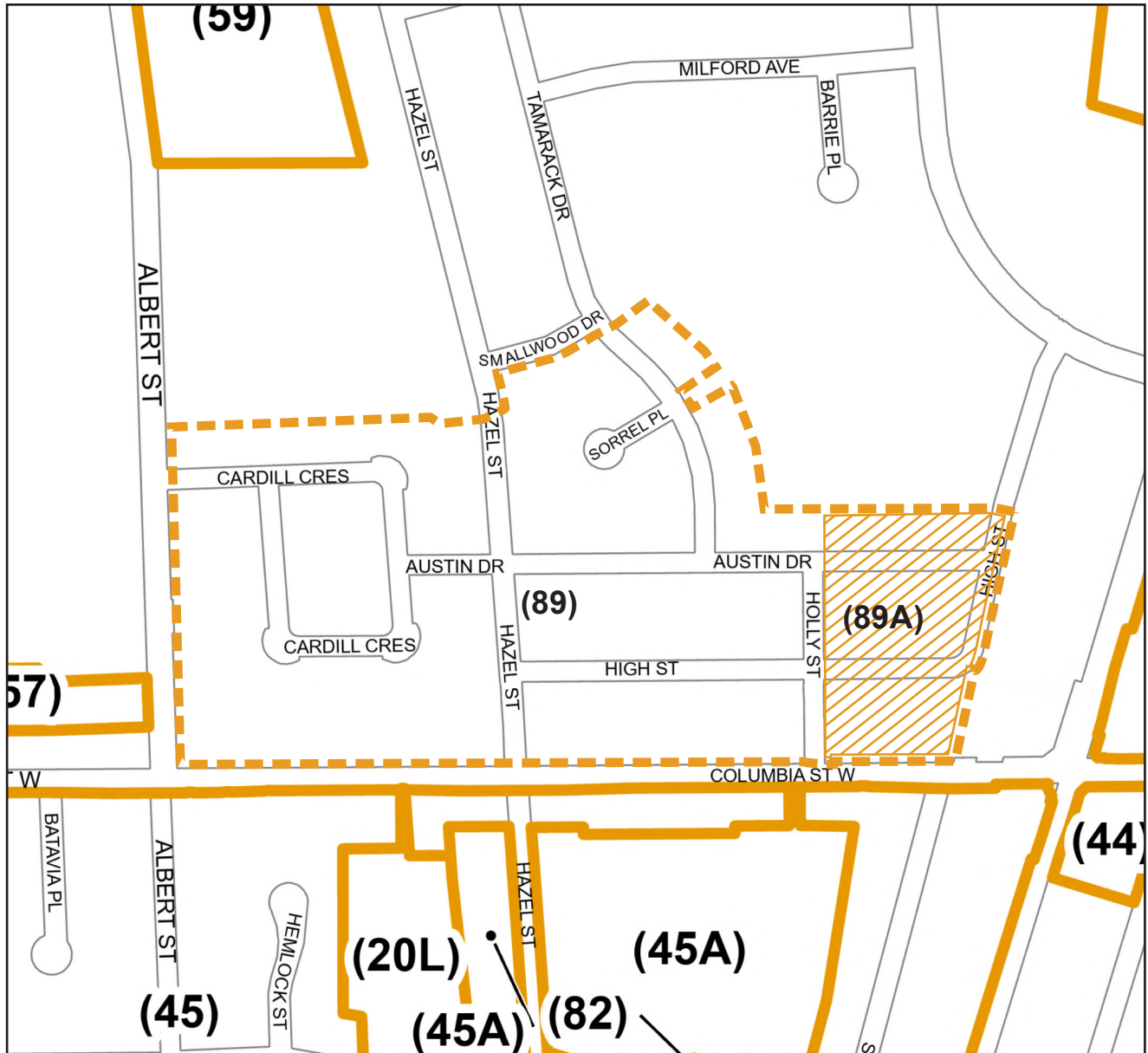


AREA SPECIFIC POLICY 89A



N.T.S

MAP 6  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**APPENDIX AREA SPECIFIC POLICIES**



AREA SPECIFIC POLICY 89

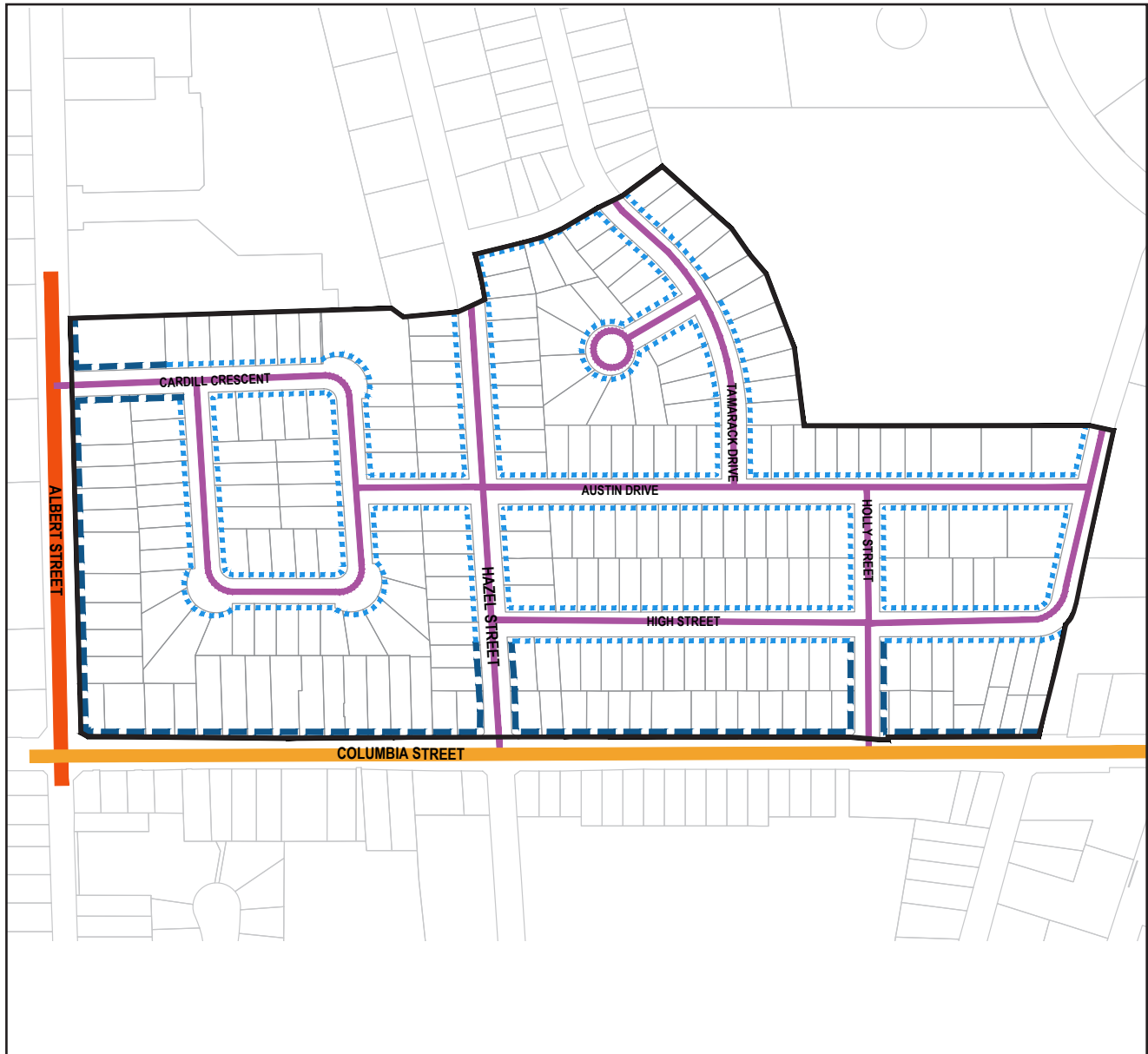


AREA SPECIFIC POLICY 89A



N.T.S

MAP 7  
**AMENDMENT NO. \_\_\_\_**  
 TO THE  
**CITY OF WATERLOO OFFICIAL PLAN**  
**SCHEDULE A45a STREET FRONTAGE AREAS**  
**AND STREET ELEMENTS**



- MAIN STREET TO BE ADDED
- MIXED-USE STREET TO BE ADDED
- RESIDENTIAL STREET TO BE ADDED
- CONVERTIBLE FRONTAGE AREAS TO BE ADDED
- NEIGHBOURHOOD FRONTAGE AREAS TO BE ADDED



N.T.S

# RESIDENTIAL SUGARBUSH SIX (RS-6)

## Zone

7.21 Residential Sugarbush Six (RS-6) Zone

## Permitted Uses

7.21.1 No PERSON shall erect, alter, enlarge, reconstruct, locate, or use any BUILDING or STRUCTURE in whole or in part, nor use any land in whole or in part, in the Residential Sugarbush Six (RS-6) zone for any purpose other than one or more of the following permitted uses:

### 7.21.1.1 Primary Uses:

- a.) MULTI-UNIT RESIDENTIAL BUILDING (including APARTMENT BUILDING)
- b.) MIXED USE BUILDING with DWELLING UNITS above the FIRST STOREY

### 7.21.1.2 Primary Uses:

- a.) TOWNHOUSE BUILDING
- b.) FREEHOLD TOWNHOUSE BUILDING
- c.) STACKED TOWNHOUSE BUILDING

### 7.21.1.3 Complementary Uses:

- a.) MUNICIPAL RECREATIONAL FACILITY
- b.) PRIVATE SCHOOL ♦
- c.) SPIRITUAL USE ♦ (see section 3.S.4)

### 7.21.1.4 Ancillary Uses, meaning the uses are subordinate and incidental to a permitted DWELLING UNIT:

- a.) HOME OCCUPATION (see section 3.H.3 and Table 6A)

### 7.21.1.5 Ancillary Uses, meaning the uses are subordinate and incidental to a:

- MULTI-UNIT RESIDENTIAL BUILDING
- MIXED USE BUILDING with DWELLING UNITS above the FIRST STOREY
- STACKED TOWNHOUSE BUILDING
- a.) BAKE SHOP ♦
- b.) CAFE ♦
- c.) CHILD CARE CENTRE ♦
- d.) MEDICAL CLINIC ♦
- e.) OFFICE ♦
- f.) PERSONAL SERVICE SHOP ♦
- g.) PHARMACEUTICAL DISPENSARY ♦
- h.) RETAIL STORE (excluding DEPARTMENT STORES, Home Improvement Store, Automotive Supply Stores) ♦
- i.) VARIETY STORE ♦

### 7.21.1.6 Ancillary Uses, meaning the uses are subordinate and incidental to a BUILDING:

- a.) Hydro Transformer Room

7.21.2 Notwithstanding anything to the contrary, the symbol "♦" means that the Complementary Use or Ancillary Use is not permitted on a LOT that ABUTS Smallwood Drive, Sorrel Place, or Tamarack Drive, excluding THROUGH LOTS where the symbol "♦" means that the Complementary Use or Ancillary Use is not permitted within 30 metres of Sorrel Place.

# CITY OF WATERLOO

## Front Lot Line and Flankage Lot Line Interpretation

7.21.3 Notwithstanding anything to the contrary, for the purposes of this zone, on a CORNER LOT the longer STREET LINE shall be deemed to be the FRONT LOT LINE, and the shorter STREET LINE shall be deemed to be the FLANKAGE LOT LINE.

## Performance Standards

7.21.4 The following regulations in Table 7V shall apply to every LOT, BUILDING and STRUCTURE in the Residential Sugarbush Six (RS-6) zone:

<b>Table 7V: Regulations – RESIDENTIAL SUGARBUSH SIX ZONE (RS-6)</b>			
		<b>Primary Uses specified in 7.21.1.1 and Complementary Uses specified in 7.21.1.3</b>	<b>Primary Uses specified in 7.21.1.2</b>
LOT AREA (minimum)		1,000 square metres	(none)
LOT FRONTAGE (minimum)		20 metres	5.5 metres
STREET LINE setback (minimum)	Convertible Frontage	5 metres	5 metres
	Neighbourhood Frontage	5 metres	5 metres
STREET LINE setback (maximum)	Convertible Frontage	At least 75% of the STREET LINE BUILDING FAÇADE shall be within 7 metres of the STREET LINE	At least 75% of the STREET LINE BUILDING FAÇADE shall be within 7 metres of the STREET LINE
	Neighbourhood Frontage	8 metres	8 metres
SIDE YARD setback (minimum)	Convertible Frontage	3 metres	3 metres
	Neighbourhood Frontage	3 metres	3 metres
REAR YARD setback (minimum)		7.5 metres	7.5 metres
Height of FIRST STOREY (minimum)	Convertible Frontage	4.0 metres	4.0 metres

# CITY OF WATERLOO

<b>Table 7V: Regulations – RESIDENTIAL SUGARBUSH SIX ZONE (RS-6)</b>			
	Neighbourhood Frontage, applicable to BUILDINGS constructed after the effective date of this BY-LAW	4.0 metres	4.0 metres
BUILDING HEIGHT (maximum)	Convertible Frontage	21.5 metres	21.5 metres
	Neighbourhood Frontage	21.5 metres	21.5 metres
Density (maximum)		450 BEDROOMS per hectare	450 BEDROOMS per hectare
Number of FIRST STOREY Entrances to the BUILDING (minimum)	Convertible Frontage	1 per 15 metres of STREET LINE BUILDING FAÇADE	1 per DWELLING UNIT
	Neighbourhood Frontage	1 per 15 metres of STREET LINE BUILDING FAÇADE	1 per DWELLING UNIT
AMENITY AREA, applicable to BUILDINGS constructed after the effective date of this BY-LAW (minimum)		3 square metres for the first BEDROOM and 2 square metres for each additional BEDROOM in the DWELLING UNIT	3 square metres for the first BEDROOM and 2 square metres for each additional BEDROOM in the DWELLING UNIT
LANDSCAPED OPEN SPACE (minimum)		30%	30%
Residential PARKING SPACES (minimum)		1 per DWELLING UNIT	1 per DWELLING UNIT
VISITOR PARKING SPACES (minimum)		0.1 per DWELLING UNIT	0.1 per DWELLING UNIT
PARKING SPACES for Non-Residential Uses (minimum) (see 7.21.12)		2.4 per 100 square metres of BUILDING FLOOR AREA	2.4 per 100 square metres of BUILDING FLOOR AREA

# CITY OF WATERLOO

<b>Table 7V: Regulations – RESIDENTIAL SUGARBUSH SIX ZONE (RS-6)</b>			
	Non-Residential Uses, applicable to BUILDINGS constructed after the effective date of this BY-LAW	1 per 100 square metres of BUILDING FLOOR AREA	1 per 100 square metres of BUILDING FLOOR AREA
Number of DRIVEWAYS per LOT (maximum)		1	1
Ancillary Uses specified in sections 7.16.1.5 and 7.16.1.6		Ancillary Uses shall not collectively exceed 15% of the BUILDING FLOOR AREA on the LOT or 5,000 square metres, whichever is more restrictive	
		The maximum BUILDING FLOOR AREA of each RETAIL STORE, CAFE or BAKE SHOP shall not exceed 465 square metres	
		Ancillary Uses shall be located on the FIRST STOREY of a MIXED USE BUILDING	Ancillary Uses shall be located on the FIRST STOREY of a STACKED TOWNHOUSE BUILDING
		Ancillary Uses shall abut the FRONT BUILDING FAÇADE or FLANKAGE BUILDING FAÇADE	
Number of main BUILDINGS per LOT (maximum)		More than one (1) permitted	More than one (1) permitted

NOTE: BICYCLE PARKING requirements are contained in section 6.6. LOADING SPACE requirements are contained in section 6.9.

7.21.5 In the case of multiple BUILDINGS on a LOT, only the BUILDING located nearest the STREET shall comply with the STREET LINE setback regulations in Table 7V.

7.21.6 Notwithstanding Table 7V, part of the BUILDING may be set back from the STREET LINE up to eight metres (8m) to accommodate an OUTDOOR CAFE PATIO or OUTDOOR BAKE SHOP PATIO.

7.21.7 Notwithstanding anything to the contrary, every BUILDING shall have a main entrance at GRADE on the FRONT BUILDING FAÇADE or FLANKAGE BUILDING FAÇADE. For the purposes of calculating the number of FIRST STOREY entrances to a BUILDING, any fraction shall be rounded to the next highest whole number.

## Street Frontages

7.21.8 For the purposes of this zone:

- a.) "Convertible Frontage" shall mean the Convertible Frontage illustrated in section 3.S.7.
- b.) "Neighbourhood Frontage" shall mean the Neighbourhood Frontage illustrated in section 3.S.7.

# CITY OF WATERLOO

---

7.21.9 Where multiple Frontages in section 7.21.8 apply to a LOT, the applicable STREET LINE setback specified in Table 7V shall apply to the associated Frontage.

7.21.10 Notwithstanding anything to the contrary, on a LOT with a Convertible Frontage, DWELLING UNITS and Non-Residential Uses shall be permitted on the FIRST STOREY.

7.21.11 Notwithstanding anything to the contrary, on a LOT with a Neighbourhood Frontage:

- a.) DWELLING UNITS shall be permitted on the FIRST STOREY.
- b.) Ancillary Uses specified in section 7.21.1.5 shall be permitted on the FIRST STOREY.
- c.) Each DWELLING UNIT located on the FIRST STOREY that abuts a STREET LINE BUILDING FAÇADE shall have an independent primary entrance to the DWELLING UNIT from the FRONT BUILDING FACADE or FLANKAGE BUILDING FACADE as the case may be.

## Parking and Parking Structures

7.21.12 Notwithstanding anything to the contrary in Table 7Q, for the following uses:

- PERSONAL SERVICE SHOP
- OFFICE
- CAFÉ

the minimum parking rate shall be two (2) PARKING SPACES per 100 square metres of BUILDING FLOOR AREA.

7.21.13 Notwithstanding anything to the contrary, PARKING SPACES shall be prohibited in the FRONT YARD and FLANKAGE YARD.

7.21.14 STRUCTURED PARKING shall be permitted on the FIRST STOREY provided that:

- a.) A minimum twenty five percent (25%) of the FIRST STOREY shall be comprised of one or more of the following uses:
  - HABITABLE FLOOR SPACE
  - circulation spaces, such as hallways, elevators, and the like
  - common indoor AMENITY AREA
  - DWELLING UNIT
  - commercial uses specified in section 7.21.1.5
  - institutional uses specified in section 7.21.1.3
  - entrance / foyers
  - hydro transformer room
  - management office
  - lobby, reception area, seating area, and the like
- b.) STRUCTURED PARKING shall be located entirely behind the BUILDING FLOOR AREA devoted to the uses specified in section 7.21.14.a.).
- c.) For an INTERIOR LOT, the BUILDING FLOOR AREA devoted to the uses specified in section 7.21.14.a.) shall abut the entire FRONT BUILDING FAÇADE.



# CITY OF WATERLOO

---

- d.) For a CORNER LOT, the BUILDING FLOOR AREA devoted to the uses specified in section 7.21.14.a.) shall abut the entire FRONT BUILDING FAÇADE and or the entire FLANKAGE BUILDING FAÇADE, provided further that:
  - i.) where the BUILDING FLOOR AREA devoted to the uses specified in section 7.21.14.a.) only abuts the FRONT BUILDING FAÇADE, the STRUCTURED PARKING abutting the FLANKAGE BUILDING FAÇADE shall be visibly screened from view from the STREET;
  - ii.) where the BUILDING FLOOR AREA devoted to the uses specified in section 7.21.14.a.) only abuts the FLANKAGE BUILDING FAÇADE, the STRUCTURED PARKING abutting the FRONT BUILDING FAÇADE shall be visibly screened from view from the STREET.
- e.) Notwithstanding anything to the contrary, where the FIRST STOREY is partially comprised of STRUCTURED PARKING, the principal BUILDING entrance shall be located on the STREET LINE BUILDING FAÇADE containing the BUILDING FLOOR AREA required in section 7.21.14.a.).

7.21.15 Notwithstanding anything to the contrary, a DRIVEWAY may comprise part of the FRONT BUILDING FAÇADE or FLANKAGE BUILDING FAÇADE.

7.21.16 Notwithstanding anything to the contrary, on a LOT within a Station Area, as shown on OFFICIAL PLAN Schedule 'J' (Station Areas), no PARKING SPACES shall be required for Residential or Non-Residential uses.

## Holding Symbol

- 7.21.17 Notwithstanding anything to the contrary, where the holding symbol (H) is applied, only those uses which lawfully existed on the date of passing of this BY-LAW shall be permitted to continue until such time as the holding symbol (H) is removed. The holding symbol (H) shall not be removed until the following criteria are satisfied:
- a.) Verification of sufficient servicing capacity (water, sanitary, and stormwater) to fully service the LOT and development thereon.
  - b.) Verification of sufficient transportation capacity and transportation infrastructure within the surrounding road network affected by the development and or use of the LOT.
  - c.) For known or suspected contaminated sites as identified by the REGION, verification from the REGION and or the Province of Ontario that the owner has completed a Record of Site Condition.
  - d.) All required clearances have been provided from a building or any other structure to Enova Power Corp. infrastructure, including overhead powerlines, to the satisfaction of the CITY.
  - e.) Potential pedestrian corridors have been secured through lands to be developed, where identified in the OFFICIAL PLAN, or alternative arrangements have been made to the satisfaction of the CITY.

# CITY OF WATERLOO

---

- f.) Verification through an urban design brief that the LOT and development thereon will conform to: (i.) the OFFICIAL PLAN; (ii.) the REGION'S Official Plan; and (iii.) the CITY'S Urban Design Manual in effect, including the Sugarbush South Urban Design Guidelines.
  - g.) Demonstrate compliance with the Provincial D-6 Guideline "Compatibility Between Industrial Facilities and Sensitive Land Uses" where there are potential land use compatibility issues associated with existing and or planned industrial uses operating in proximity to residential or other sensitive land uses.
  - h.) A detailed Noise Study has been submitted to the satisfaction of the CITY. The study shall evaluate stationary noise impacts both on the proposed development and emanating from the proposed development. All buildings on the site, and mechanical equipment thereto, shall be acoustically designed to achieve all Ministry of the Environment and Climate Change noise level objectives on- and off-site. The study shall be conducted when grading plans, elevations and floor plans are available. The Owner shall implement the recommendations of the approved study, and shall enter into an agreement (with the CITY) under the provisions of the PLANNING ACT.
- 7.21.18 Notwithstanding section 7.16.17, the holding symbol (H) shall not apply to an EXISTING DETACHED BUILDING."

## Detached Buildings

- 7.21.19 Notwithstanding anything to the contrary, the regulations in section 7.1.2 of the Residential One (R1) zone shall apply to an EXISTING DETACHED BUILDING.

# RESIDENTIAL SUGARBUSH EIGHT (RS-8)

## Zone

7.22 Residential Sugarbush Eight (RS-8) Zone

## Permitted Uses

7.22.1 No PERSON shall erect, alter, enlarge, reconstruct, locate, or use any BUILDING or STRUCTURE in whole or in part, nor use any land in whole or in part, in the Residential Sugarbush Eight (RS-8) zone for any purpose other than one or more of the following permitted uses:

### 7.22.1.1 Primary Uses:

- a.) MULTI-UNIT RESIDENTIAL BUILDING (including APARTMENT BUILDING)
- b.) MIXED USE BUILDING with DWELLING UNITS above the FIRST STOREY
- c.) ASSISTED LIVING FACILITY
- d.) LONG TERM CARE FACILITY

### 7.22.1.2 Complementary Uses:

- a.) GOVERNMENT USE
- b.) MUNICIPAL RECREATIONAL FACILITY
- c.) PRIVATE SCHOOL
- d.) SPIRITUAL USE (see section 3.S.4)

### 7.22.1.3 Ancillary Uses, meaning the uses are subordinate and incidental to a permitted DWELLING UNIT:

- a.) HOME OCCUPATION (see section 3.H.3 and Table 6A)

### 7.22.1.4 Ancillary Uses, meaning the uses are subordinate and incidental to a use permitted in section 7.12.1.1 and or section 7.12.1.2:

- a.) Art Gallery
- b.) ARTIST STUDIO (CLASS A)
- c.) BAKE SHOP
- d.) CAFE
- e.) CHILD CARE CENTRE
- f.) COMMERCIAL RECREATION
- g.) COMMERCIAL WELLNESS
- h.) ELECTRONIC GAMING CENTRE
- i.) MEDICAL CLINIC
- j.) OFFICE
- k.) PERSONAL SERVICE SHOP
- l.) PHARMACEUTICAL DISPENSARY
- m.) RESTAURANT
- n.) RESTAURANT, TAKE-OUT
- o.) RETAIL STORE (excluding DEPARTMENT STORES, Home Improvement Store, Automotive Supply Stores)
- p.) VARIETY STORE

### 7.22.1.5 Ancillary Uses, meaning the uses are subordinate and incidental to a BUILDING:

- a.) Hydro Transformer Room

## Front Lot Line and Flankage Lot Line Interpretation

# CITY OF WATERLOO

7.22.2 Notwithstanding anything to the contrary, for the purposes of this zone, on a CORNER LOT the longer STREET LINE shall be deemed to be the FRONT LOT LINE, and the shorter STREET LINE shall be deemed to be the FLANKAGE LOT LINE.

## Performance Standards

7.22.3 The following regulations in Table 7W shall apply to every LOT, BUILDING and STRUCTURE in the Residential Sugarbush Eight (RS-8) zone:

<b>Table 7W: Regulations – RESIDENTIAL SUGARBUSH EIGHT ZONE (RS-8)</b>		
LOT AREA (minimum)		1,000 square metres
LOT FRONTAGE (minimum)		20 metres
STREET LINE setback (minimum)	Convertible Frontage	5 metre
	Neighbourhood Frontage	5 metres
STREET LINE setback (maximum)	Convertible Frontage	At least 75% of the STREET LINE BUILDING FAÇADE shall be within 7 metres of the STREET LINE
	Neighbourhood Frontage	8 metres
SIDE YARD setback (minimum)	Convertible Frontage	3 metres
	Neighbourhood Frontage	3 metres
REAR YARD setback (minimum)		7.5 metres
Height of FIRST STOREY (minimum)	Convertible Frontage	4.0 metres
	Neighbourhood Frontage, applicable to BUILDINGS constructed after the effective date of this BY-LAW	4.0 metres
BUILDING HEIGHT (minimum)		10.5 metres
BUILDING HEIGHT (maximum)	Convertible Frontage	27.5 metres
	Neighbourhood Frontage	27.5 metres
PODIUM Height (minimum)		10.5 metres
PODIUM Height (maximum)		20 metres

# CITY OF WATERLOO

<b>Table 7W: Regulations – RESIDENTIAL SUGARBUSH EIGHT ZONE (RS-8)</b>		
BUILDING STEPBACK above PODIUM (minimum)		3 metres
Density (maximum)		450 BEDROOMS per hectare
Number of FIRST STOREY Entrances to the BUILDING (minimum)	Convertible Frontage	1 per 15 metres of STREET LINE BUILDING FAÇADE
	Neighbourhood Frontage	1 per 15 metres of STREET LINE BUILDING FAÇADE
AMENITY AREA, applicable to BUILDINGS constructed after the effective date of this BY-LAW (minimum)		3 square metres for the first BEDROOM and 2 square metres for each additional BEDROOM in the DWELLING UNIT
LANDSCAPED OPEN SPACE (minimum)		30%
Residential PARKING SPACES (minimum)		1 per DWELLING UNIT
VISITOR PARKING SPACES (minimum)		0.1 per DWELLING UNIT
PARKING SPACES for Non-Residential Uses (minimum) (see 7.17.11)		2.4 per 100 square metres of BUILDING FLOOR AREA
Number of DRIVEWAYS per LOT (maximum)		1
Ancillary Uses specified in sections 7.12.1.4 and 7.12.1.5		Ancillary Uses shall not collectively exceed 15% of the BUILDING FLOOR AREA on the LOT or 5,000 square metres, whichever is more restrictive
		The maximum BUILDING FLOOR AREA of each RETAIL STORE, CAFÉ, BAKE SHOP or RESTAURANT (including TAKE-OUT RESTAURANT) shall not exceed 465 square metres
Ancillary Uses specified in sections 7.12.1.4 and 7.12.1.5		Ancillary Uses shall be located on the FIRST STOREY of a MIXED USE BUILDING
		Ancillary Uses shall abut the FRONT BUILDING FACADE or FLANKAGE BUILDING FACADE
Number of main BUILDINGS per LOT (maximum)		More than one (1) permitted

NOTE: BICYCLE PARKING requirements are contained in section 6.6. LOADING SPACE requirements are contained in section 6.9.

# CITY OF WATERLOO

---

- 7.22.4 In the case of multiple BUILDINGS on a LOT, only the BUILDING located nearest the STREET shall comply with the STREET LINE setback regulations in Table 7W.
- 7.22.5 Notwithstanding Table 7W, part of the BUILDING may be set back from the STREET LINE up to eight metres (8m) to accommodate an OUTDOOR CAFE PATIO, OUTDOOR RESTAURANT PATIO, or OUTDOOR BAKE SHOP PATIO.
- 7.22.6 Notwithstanding anything to the contrary, every BUILDING shall have a main entrance at GRADE on the FRONT BUILDING FAÇADE or FLANKAGE BUILDING FAÇADE. For the purposes of calculating the number of FIRST STOREY entrances to a BUILDING, any fraction shall be rounded to the next highest whole number.

## Street Frontages

- 7.22.7 For the purposes of this zone:
- a.) "Convertible Frontage" shall mean the Convertible Frontage illustrated in section 3.S.7.
  - b.) "Neighbourhood Frontage" shall mean the Neighbourhood Frontage illustrated in section 3.S.7.
- 7.22.8 Where multiple Frontages in section 7.17.7 apply to a LOT, the applicable STREET LINE setback specified in Table 7W shall apply to the associated Frontage.
- 7.22.9 Notwithstanding anything to the contrary, on a LOT with a Convertible Frontage, DWELLING UNITS and Non-Residential Uses shall be permitted on the FIRST STOREY.
- 7.22.10 Notwithstanding anything to the contrary, on a LOT with a Neighbourhood Frontage:
- a.) DWELLING UNITS shall be permitted on the FIRST STOREY.
  - b.) Ancillary Uses specified in section 7.11.1.4 shall be permitted on the FIRST STOREY.
  - c.) Each DWELLING UNIT located on the FIRST STOREY that abuts a STREET LINE BUILDING FAÇADE shall have an independent primary entrance to the DWELLING UNIT from the FRONT BUILDING FACADE or FLANKAGE BUILDING FACADE as the case may be.

## Parking and Parking Structures

- 7.22.11 Notwithstanding anything to the contrary in Table 7W, for the following uses:
- PERSONAL SERVICE SHOP
  - OFFICE
  - CAFÉ
  - COMMERCIAL RECREATION
  - COMMERCIAL WELLNESS
- the minimum parking rate shall be two (2) PARKING SPACES per 100 square metres of BUILDING FLOOR AREA.
- 7.22.12 Notwithstanding anything to the contrary, PARKING SPACES shall be prohibited in the FRONT YARD and FLANKAGE YARD.

# CITY OF WATERLOO

---

7.22.13 STRUCTURED PARKING shall be permitted on the FIRST STOREY provided that:

- a.) A minimum twenty five percent (25%) of the FIRST STOREY shall be comprised of one or more of the following uses:
  - HABITABLE FLOOR SPACE
  - circulation spaces, such as hallways, elevators, and the like
  - common indoor AMENITY AREA
  - DWELLING UNIT
  - commercial uses specified in section 7.17.1.4
  - institutional uses specified in section 7.17.1.2
  - entrance / foyers
  - hydro transformer room
  - management office
  - lobby, reception area, seating area, and the like
- b.) STRUCTURED PARKING shall be located entirely behind the BUILDING FLOOR AREA devoted to the uses specified in section 7.17.13.a.).
- c.) For an INTERIOR LOT, the BUILDING FLOOR AREA devoted to the uses specified in section 7.17.13.a.) shall abut the entire FRONT BUILDING FAÇADE.
- d.) For a CORNER LOT, the BUILDING FLOOR AREA devoted to the uses specified in section 7.17.13.a.) shall abut the entire FRONT BUILDING FAÇADE and or the entire FLANKAGE BUILDING FAÇADE, provided further that:
  - i.) where the BUILDING FLOOR AREA devoted to the uses specified in section 7.17.13.a.) only abuts the FRONT BUILDING FAÇADE, the STRUCTURED PARKING abutting the FLANKAGE BUILDING FAÇADE shall be visibly screened from view from the STREET;
  - ii.) where the BUILDING FLOOR AREA devoted to the uses specified in section 7.17.13.a.) only abuts the FLANKAGE BUILDING FAÇADE, the STRUCTURED PARKING abutting the FRONT BUILDING FAÇADE shall be visibly screened from view from the STREET.
- e.) Notwithstanding anything to the contrary, where the FIRST STOREY is partially comprised of STRUCTURED PARKING, the principal BUILDING entrance shall be located on the STREET LINE BUILDING FAÇADE containing the BUILDING FLOOR AREA required in section 7.17.13.a.).

7.22.14 Notwithstanding anything to the contrary, a DRIVEWAY may comprise part of the FRONT BUILDING FAÇADE or FLANKAGE BUILDING FAÇADE.

7.22.15 Notwithstanding anything to the contrary, on a LOT within a Station Area, as shown on OFFICIAL PLAN Schedule 'J' (Station Areas), no PARKING SPACES shall be required for Residential or Non-Residential uses.

## Holding Symbol

7.22.16 Notwithstanding anything to the contrary, where the holding symbol (H) is applied, only those uses which lawfully existed on the date of passing of this BY-LAW shall be permitted to continue

# CITY OF WATERLOO

---

until such time as the holding symbol (H) is removed. The holding symbol (H) shall not be removed until the following criteria are satisfied:

- a.) Verification of sufficient servicing capacity (water, sanitary, and stormwater) to fully service the LOT and development thereon.
- b.) Verification of sufficient transportation capacity and transportation infrastructure within the surrounding road network affected by the development and or use of the LOT.
- c.) For known or suspected contaminated sites as identified by the REGION, verification from the REGION and or the Province of Ontario that the owner has completed a Record of Site Condition.
- d.) All required clearances have been provided from a building or any other structure to Enova Power Corp. infrastructure, including overhead powerlines, to the satisfaction of the CITY.
- e.) Potential pedestrian corridors have been secured through lands to be developed, where identified in the OFFICIAL PLAN, or alternative arrangements have been made to the satisfaction of the CITY.
- f.) Verification through an urban design brief that the LOT and development thereon will conform to: (i.) the OFFICIAL PLAN; (ii.) the REGION'S Official Plan; and (iii.) the CITY'S Urban Design Manual in effect, including the Sugarbush South Urban Design Guidelines.
- g.) Demonstrate compliance with the Provincial D-6 Guideline "Compatibility Between Industrial Facilities and Sensitive Land Uses" where there are potential land use compatibility issues associated with existing and or planned industrial uses operating in proximity to residential or other sensitive land uses.
- h.) A detailed Noise Study has been submitted to the satisfaction of the CITY. The study shall evaluate stationary noise impacts both on the proposed development and emanating from the proposed development. All buildings on the site, and mechanical equipment thereto, shall be acoustically designed to achieve all Ministry of the Environment and Climate Change noise level objectives on- and off-site. The study shall be conducted when grading plans, elevations and floor plans are available. The Owner shall implement the recommendations of the approved study, and shall enter into an agreement (with the CITY) under the provisions of the PLANNING ACT.

7.22.17 Notwithstanding section 7.22.16, the holding symbol (H) shall not apply to an EXISTING DETACHED BUILDING.



# CITY OF WATERLOO

---

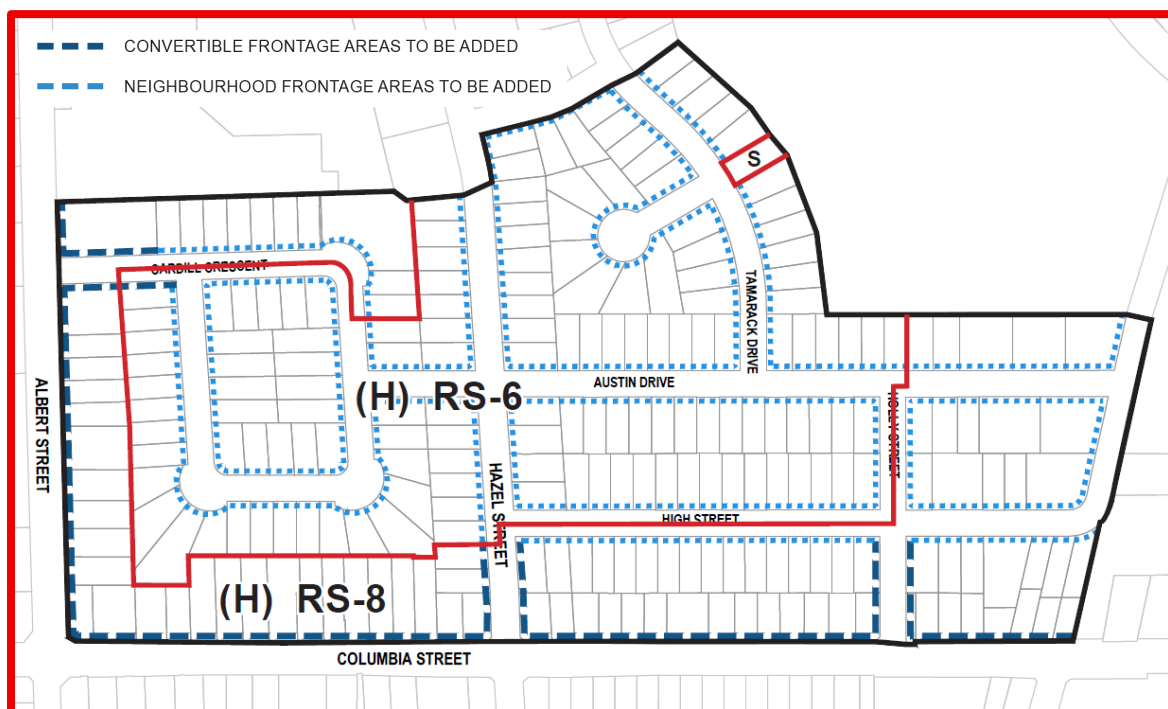
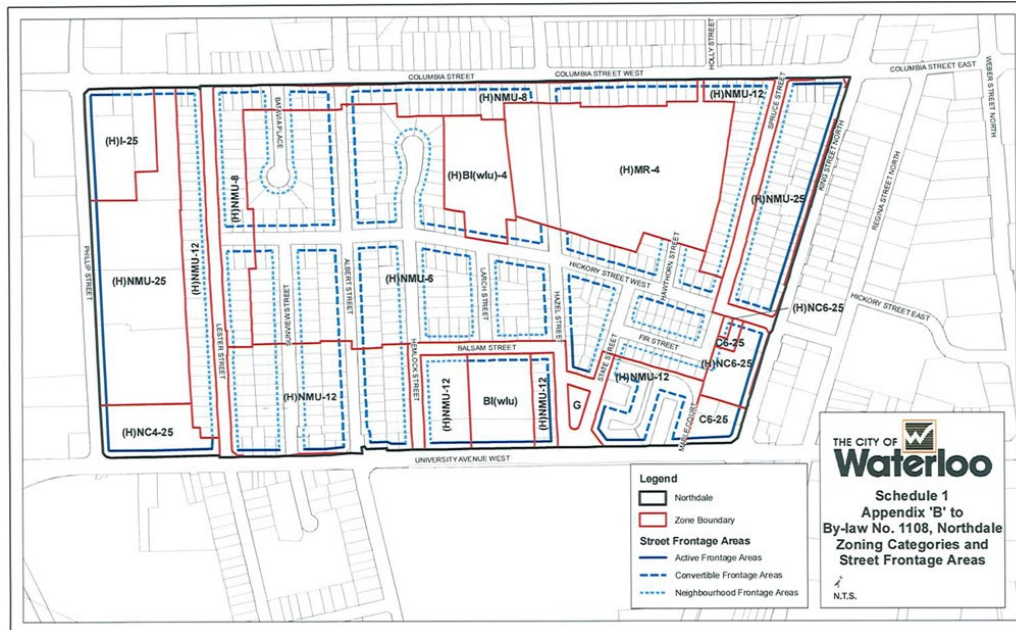
## Detached Buildings

7.22.18 Notwithstanding anything to the contrary, the regulations in section 7.1.2 of the Residential One (R1) zone shall apply to an EXISTING DETACHED BUILDING.

**S**

### 3.S.1 STREET FRONTAGES – NORTHDALÉ & SUGARBUSH SOUTH

3.S.8.1 The Street Frontages identified on the inset map shall apply to the RN-6, RS-6, RN-8, RS-8, RN-12 and RN-25 zones:



**SCHEDULE 'C'**  
SITE SPECIFIC ZONING

Exception	Properties	Zoning	File Reference
C68	Peppler St. & Elgin St. area	R4	By-law 2004-142

Location: Peppler St. & Elgin St. as shown on Schedule 'C1' to this BY-LAW.

Site Specific Regulations:

- a) Notwithstanding anything to the contrary, on the lands illustrated as "Subject Lands" on Image 1 hereto, the following USES shall not be permitted:
  - i. DUPLEX BUILDINGS
  - ii. SEMI-DETACHED BUILDINGS
  - iii. FREEHOLD SEMI-DETACHED BUILDINGS
- b) Notwithstanding anything to the contrary, a SECOND RESIDENTIAL UNIT is permitted within a SINGLE DETACHED DWELLING subject to the regulations of 3.S.1.

Image 1: Subject Lands



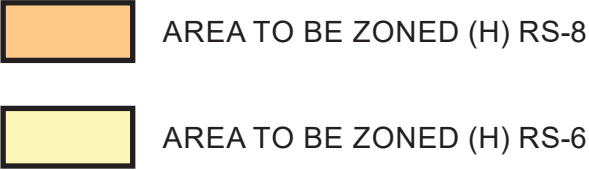
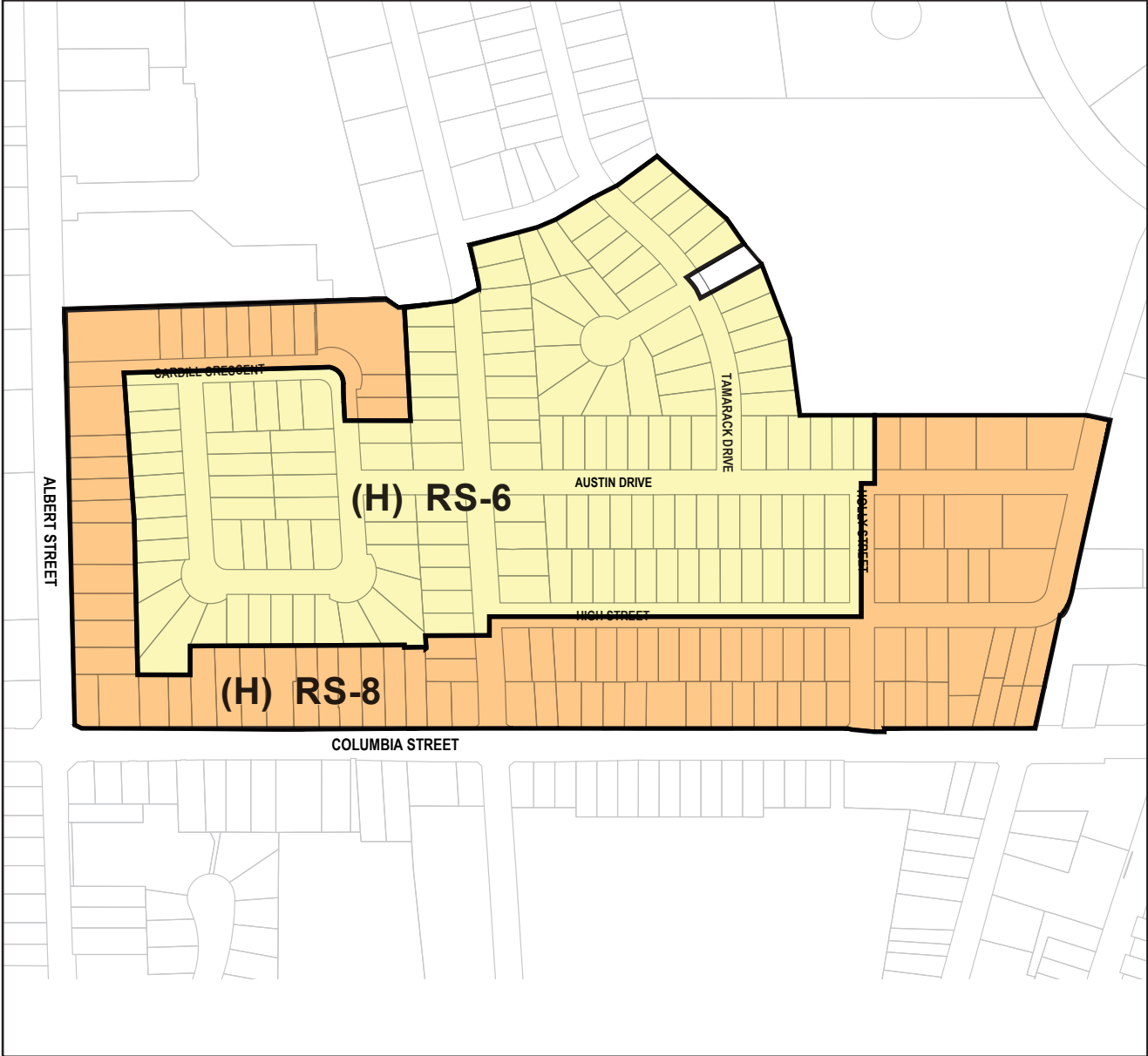
<b>Exception</b>	<b>Properties</b>	<b>Zoning</b>	<b>File Reference</b>
C288	Austin Dr., High St. & Holly St. area	(H)RS-8	By-law 2026-##

Location: Austin Dr., High St. & Holly St. area  
as shown on Schedule 'C1' to this BY-LAW.

Site Specific Regulations:

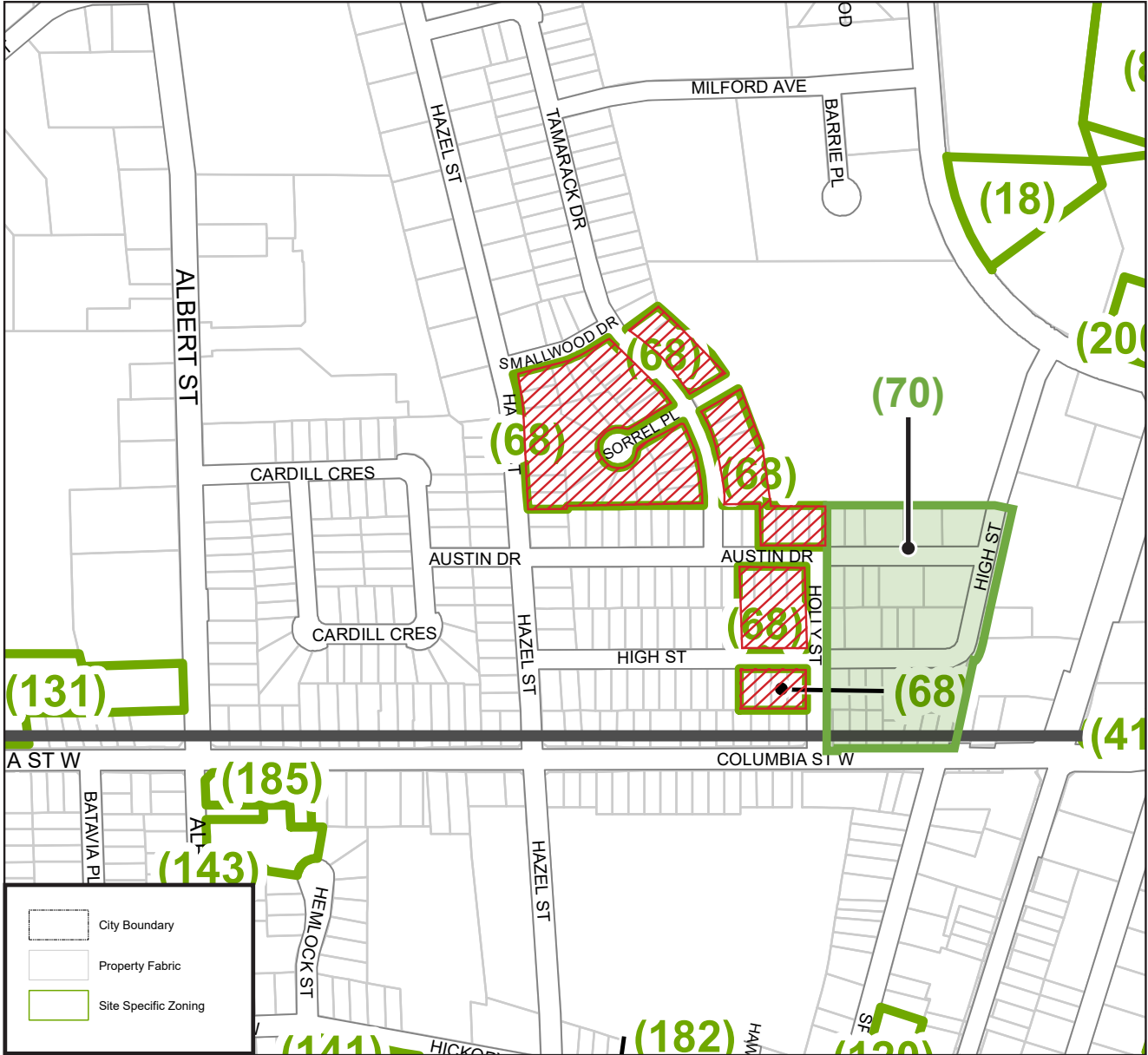
- a) That a holding (H) symbol shall be applied to the lands. Until the holding (H) symbol is removed, the maximum BUILDING HEIGHT shall be 21.5 metres and 6 STOREYS. The holding (H) symbol shall not be removed until:
  - i. The CITY passes a by-law under Section 99.1 of the Municipal Act, 2001 to prohibit and/or regulate the demolition and conversion of residential rental properties with six or more units to a purpose other than the purpose of a residential rental property.



MAP 1  
AMENDMENT NO. \_\_\_\_  
TO THE  
CITY OF WATERLOO ZONING BY-LAW NO.2018-50  
SCHEDULE A



N.T.S

MAP 2  
AMENDMENT NO. \_\_\_\_  
TO THE  
CITY OF WATERLOO ZONING BY-LAW NO.2018-50  
SCHEDULE C1 - SITE SPECIFIC ZONING

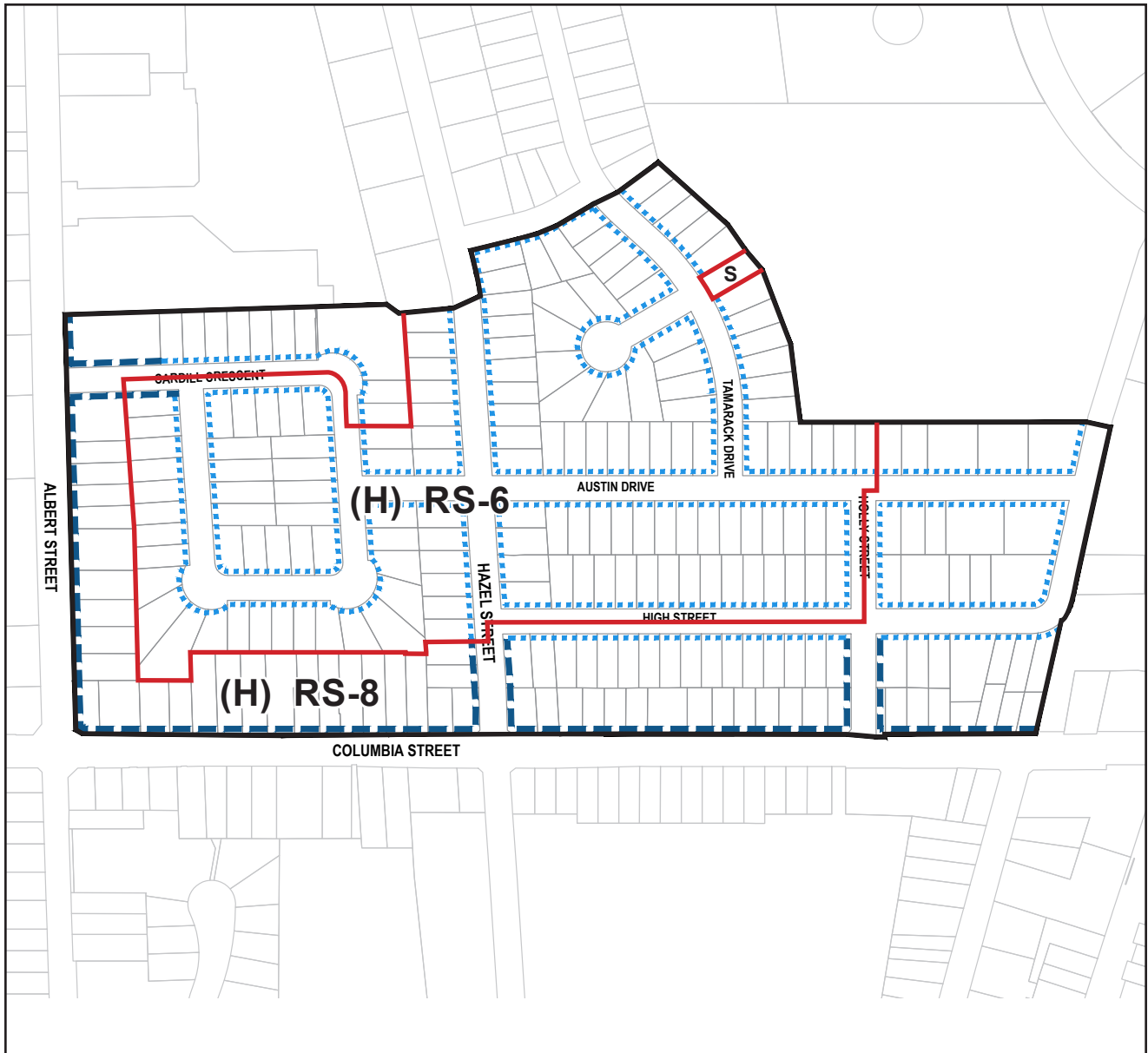






-  NEW EXCEPTION C70 TO BE ADDED
-  EXCEPTION C68 TO BE REMOVED



MAP 3  
AMENDMENT NO. \_\_\_\_

TO THE  
**CITY OF WATERLOO ZONING BY-LAW NO.2018-50**  
Schedule 1 Appendix 'B' to By-law No. 1109, Northdale and Sugarbush  
South Zoning Categories and Street Frontage Area



-  SUGARBUSH SOUTH BOUNDARY
-  ZONE BOUNDARY
-  CONVERTIBLE FRONTAGE AREAS TO BE ADDED
-  NEIGHBOURHOOD FRONTAGE AREAS TO BE ADDED



N.T.S





# Sugarbush South Urban Design Guidelines

September 2025



# Sugarbush South Urban Design Guidelines

September 2025

DRAFT

Prepared for  
**City of Waterloo**

Prepared by  
**GSP Group**  
72 Victoria Street South - Suite 201  
Kitchener, ON N2G 4Y9

# TABLE OF CONTENTS

<b>1.0 INTRODUCTION</b>	<b>6</b>
1.1 Sugarbush South Urban Design Guidelines	6
1.2 Purpose	7
1.3 Urban Design Guiding Framework	9
<b>2.0 NEIGHBOURHOOD FRAMEWORK</b>	<b>11</b>
2.1 Vision	11
2.2 Preferred Neighbourhood Elements	12
2.3 Structural Plan	15
2.4 Sustainable Design	18
<b>3.0 SITE DESIGN</b>	<b>19</b>
3.1 Building Orientation and Siting	19
3.2 Site Typologies	22
3.3 Convertible Street Frontage	29
3.4 Neighbourhood Street Frontage	33
3.5 Access and Parking	39
3.6 Site Servicing, Waste and Utilities	43

---

<b>4.0 BUILDING DESIGN</b>	<b>46</b>
----------------------------	-----------

4.1 Building Massing and Scale	46
--------------------------------	----

4.2 Streetwall Design	50
-----------------------	----

4.3 Transition to Buildings	52
-----------------------------	----

4.4 Façade Design and Articulation	54
------------------------------------	----

4.5 Private and Common Amenity Areas	57
--------------------------------------	----

4.6 Rooftop Mechanical and Mechanical Systems	60
---	----

<b>5.0 PUBLIC REALM</b>	<b>61</b>
-------------------------	-----------

5.1 Streetscape Design	61
------------------------	----

5.2 Mid-Block Connections	65
---------------------------	----

5.3 Parks and Open Space	68
--------------------------	----

5.4 Landscape Integration and Urban Canopy	71
--	----

5.5 Public Art, Identity, and Placemaking	72
---	----

<b>6.0 IMPLEMENTATION</b>	<b>74</b>
---------------------------	-----------

6.1 Planning and Development Approvals	74
--	----





# 1.0 INTRODUCTION

---

## 1.1 Sugarbush South Urban Design Guidelines

The Sugarbush South Urban Design Guidelines have been prepared in support of the City of Waterloo's broader housing objectives under the Housing Accelerator Fund (HAF). These guidelines are a key component of the planning framework intended to enable new "missing middle" and transit-supportive housing opportunities in Sugarbush South ("Sugarbush South").

In this context, "missing middle" refers to housing forms such as townhouses, stacked townhouses, and medium rise apartment buildings generally ranging from 3 to 8 storeys. These building types provide alternatives that are larger than single detached dwellings but smaller in scale than high-rise towers. These guidelines provide a framework to guide the evolution of Sugarbush South into a medium rise neighbourhood that accommodates intensification while reinforcing human scale, walkability, and community identity.

The Sugarbush South Area Specific Policy (ASP 89) supplements this framework with more detailed direction tailored to the neighbourhood's context, opportunities, and constraints. Sugarbush South, identified as Specific Provision Area 89 on Schedule "A6" of the Official Plan, is located within Waterloo's Built-Up Area and bounded by Columbia Street West to the south, Albert Street to the west, Smallwood Drive to the north, and High Street to the east. See **Figure 1: Sugarbush South Context and Extent**.

Sugarbush South is approximately 22 hectares in area and is located near two Major Transit Station Areas, three post-secondary institutions, the Northdale neighbourhood, the King Street North Major Corridor, and other services and amenities. The neighbourhood was originally developed in 1960s containing primarily low rise residential dwellings. It supports a varied demographic and plays an important role in providing rental housing within the community.

Over time, Sugarbush South will transition from a primarily low-density residential area to a vibrant, compact, and sustainable mixed-use neighbourhood that supports active transportation and transit-oriented development. Medium-rise buildings, missing middle housing forms, and context-sensitive intensification will be introduced to reinforce a cohesive urban structure, enhance streetscapes, and provide a greater diversity of housing options.

These Guidelines should be read in conjunction with the City of Waterloo Urban Design Manual, the Zoning By-law, applicable Official Plan policies, and the Comprehensive Engineering and Landscape Manual (CELM) standards. They address matters unique to Sugarbush South, including built form transitions, mid-block connectivity, and public realm enhancements, with the aim of delivering development that is contextually responsive, human-scaled, and environmentally responsible.

As the neighbourhood redevelops incrementally, these guidelines will help implement the City's vision for Sugarbush South: a complete, connected, and attractive community with active, pedestrian-oriented main streets, a network of high-quality public spaces, and buildings that contribute positively to both the neighbourhood and the city as a whole.

## 1.2 Purpose

The Sugarbush South Urban Design Guidelines set out a clear and practical framework to guide the neighbourhood's evolution into a compact, well-designed, and livable community. They are intended to:

- Establish a high standard of urban, architectural, and landscape design that reflects Sugarbush South's unique context and character.
- Translate the vision and policies of Area Specific Policy 89 into design directions that can be applied consistently to both public and private development.
- Shape the neighbourhood's transition toward a medium rise, mixed-use community while respecting existing assets such as the mature tree canopy and established residential areas.
- Provide clear expectations for built form, streetscapes, open spaces, and sustainable design practices to inform development proposals and municipal review.
- Encourage active, safe, and accessible connections within the neighbourhood and to surrounding areas, supporting walking, cycling, and transit use.
- Provide guidance to residents, property owners, and other interested parties, particularly land developers, planners, urban designers, architects, landscape architects, and City staff in the creation and evaluation of development proposals.



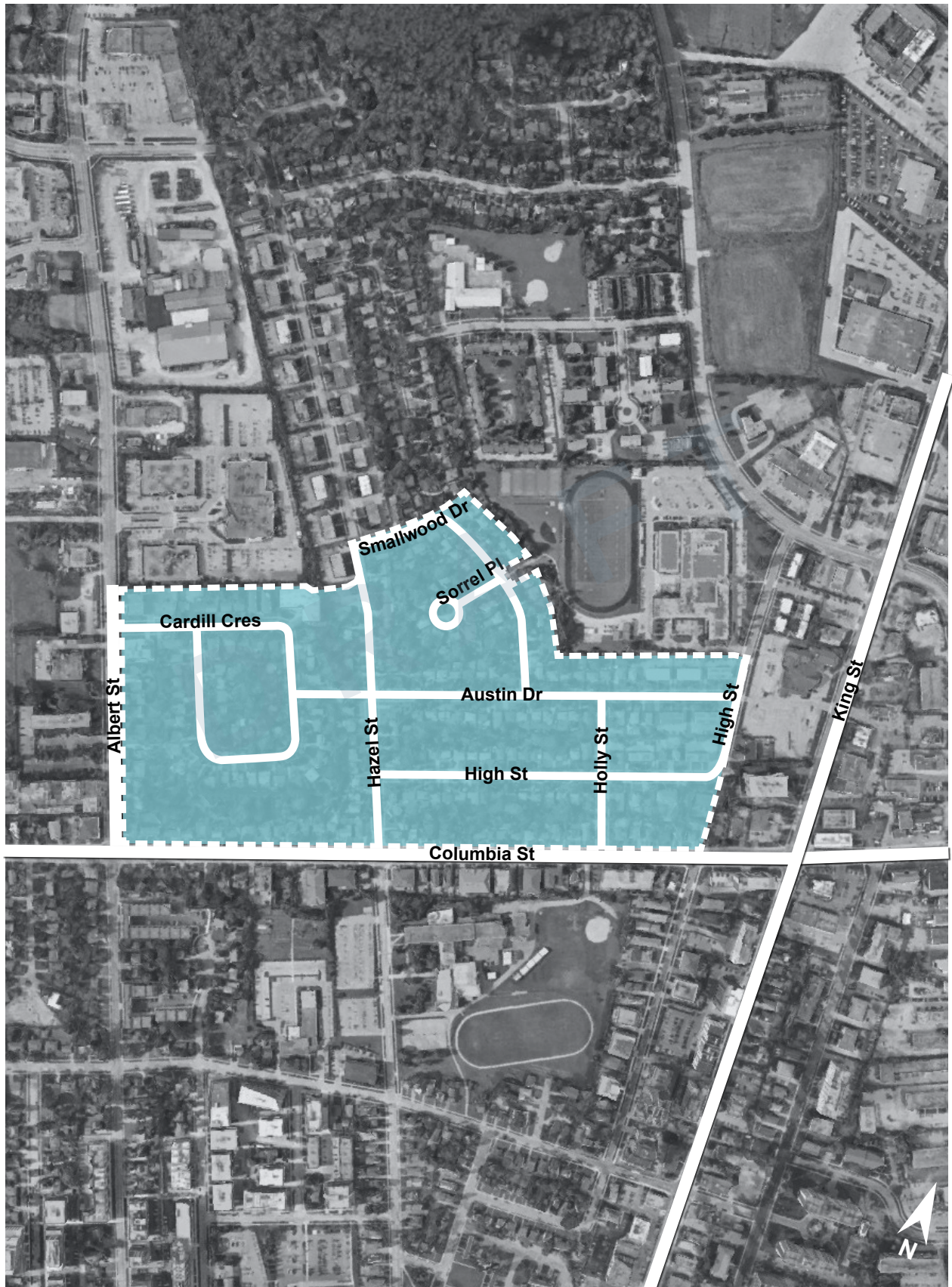


Fig.1: Sugarbush South Context and Extent



## 1.3 Urban Design Guiding Framework

### 1.3.1 Urban Design Manual

The primary purpose of the Urban Design Manual is to implement the City's urban design objectives and policies through the development review process and for select public realm improvement projects. The Manual is based on specific policy objectives established in the Urban Design section of the City's Official Plan, as outlined in Section 2.3.3.4 and the Urban Design Objectives are to:

- Promote a high standard of urban design;
- Respect context and promote *sense of place*;
- Enhance connectivity and interaction;
- Promote creativity and innovation; and,
- Encourage sustainable design.

### 1.3.2 City-Initiated Guideline

The Sugarbush South Urban Design Guidelines apply specifically to the Sugarbush South neighbourhood and are intended to be used in conjunction with the City's Urban Design Guidelines. Both sets of guidelines should be consulted in the development and review of project designs within the neighbourhood. Where there is a conflict between the two, the Sugarbush South Urban Design Guidelines take precedence. These guidelines have been prepared to align closely with the Sugarbush South Zoning By-law Amendment, which translates the principles of the Sugarbush South Official Plan Amendment into the quantitative framework that shapes the neighbourhood's form and structure.

## 1.4 How to Use This Document

### 1.4.1 Document Structure

These Guidelines are organized into the following sections:

- 1. Introduction** - Outlines the purpose, context, and guiding framework for the Sugarbush South Urban Design Guidelines, and explains their role, application, and interpretation in guiding neighbourhood intensification.
- 2. Vision** - Establishes the long-term goals, preferred neighbourhood elements, and preferred structure plan.
- 3. Site Design** - Provides direction on building orientation, site typologies, street frontages, access, parking, and site servicing.
- 4. Building** - Addresses massing, scale, transitions, streetwall design, façade articulation, amenity areas, and rooftop systems.
- 5. Public Realm** - Establishes standards for streetscapes, sidewalks, mid-block connections, parks, open spaces, landscaping, urban canopy, and placemaking elements.
- 6. Implementation** - Explains how the Guidelines will be applied during the development review process.

### 1.4.2 Applying the Guidelines

These Guidelines are intended to assist development proponents, City staff, and the public in shaping and evaluating future development in Sugarbush South.

- Development proponents should use the Guidelines to inform the preparation of development concepts and applications that align with the City's vision and expectations.
- City staff will refer to the Guidelines during the review and evaluation of development proposals, ensuring consistency with urban design objectives and supporting a coordinated planning approach.
- Community members may use the Guidelines to understand how new development will address compatibility, transitions, and improvements to the public realm.

## 2.0 NEIGHBOURHOOD FRAMEWORK

---

### 2.1 Vision

Sugarbush South is a neighbourhood in transition, poised to play a key role in accommodating a share of the City's medium rise intensification. Given the proximity of the neighbourhood to the post-secondary institutions, designated Nodes and Corridors and two Major Transit Station Areas (MTSAs), it is a priority area for growth and reinvestment.

The vision for Sugarbush South is to evolve into a vibrant, sustainable, and mixed-use community that offers a complete neighbourhood experience. It will provide a diverse range of housing types, support commercial uses within Minor Corridors, and foster a well-connected public realm that prioritizes active transportation. Medium-rise densities will be integrated thoughtfully, enhancing the character of the area while supporting a walkable, transit-oriented, and inclusive urban environment.

*PHOTO TO BE ADDED FROM THE  
PUBLIC ENGAGEMENT*

## 2.2 Preferred Neighbourhood Elements

A number of preferred neighbourhood elements have been identified which provide more detailed planning and land use objectives to guide redevelopment in Sugarbush South. The preferred neighbourhood elements below provide the basis for the land use plan for Sugarbush South, which is implemented through the land use designations and policies of the Official Plan, the implementing Zoning By-law, the City's Urban Design Manual and the Sugarbush South Urban Design Guidelines. The following preferred neighbourhood elements provide guidance to the redevelopment of Sugarbush South.

### Neighbourhood Structure:

- NS.1** The City will favour a transition from a primarily low-density neighbourhood to a medium rise residential community with an emphasis on enabling medium rise housing forms such as townhouses and medium rise apartments.
- NS.2** New development will be focused on medium rise multiple residential development, transit and pedestrian supportive principles with mixed uses primarily along Minor Corridors, allowing for a diversity of people and activities.
- NS.3** New development will consider the provision of affordable and/or attainable housing options.
- NS.4** Clear land use, urban design and built form policies, regulations and guidelines will guide future development. Building height and density will generally be greater on the periphery of the neighbourhood along Columbia Street and Albert Street, and transition towards to medium rise building forms at lower heights in the interior of the neighbourhood.

### Buildings & Development:

- BD.1** New buildings, enhancements to the public realm and related development elements should be constructed in a sustainable manner, conserving energy and resources, using durable materials and where feasible and appropriate, consider principles and strategies of net-zero design.
- BD.2** Redevelopment will be encouraged to be consistent and mindful of the existing street edge by establishing complementary front yard and street line setbacks and urban design/built form policies and guidelines to create a sense of an active and vibrant streetscape, limit breaks in the active street facade, and maximize the principles of "eyes on the street".

- BD.3** Appropriate and compatible building design techniques will be used, including transition in building heights, step-backs, and building articulation.
- BD.4** The location of ground-floor windows, doors, and main entranceways will support visibility and transparency at grade, and access from the street.
- BD.5** Indoor common amenity areas will be required and outdoor common amenity areas will be encouraged for all housing developments to promote a healthy social environment both within buildings and along the street. Outdoor elements could include porches and patios adjacent to the street, private balconies with appropriate privacy design considerations and small common courtyard and/or amenity areas that provide for connectivity to the public realm. Indoor common amenity areas may include recreational, study and living areas.
- BD.6** Parking areas will not be permitted in the front yard, and rear yard parking will be preferred. Underground or structured parking will be encouraged for higher density building forms.

#### **Public Realm:**

- PR.1** Convertible street frontage areas are identified along Columbia Street West, Albert Street and a portion of Cardill Crescent and will be designed to be convertible from residential to an animated retail, commercial or community use at a time when it can be supported.
- PR.2** The neighbourhood contains an existing mature tree canopy, typical in low rise residential neighbourhoods, and consists primarily of street trees in the right-of-way and privately owned trees in the yards of properties. Through redevelopment, every effort will be made to preserve the existing tree canopy to the extent possible or to maximize opportunities for new trees or other landscaping measures. This may include the strategic location of access driveways (including utilizing existing accesses where possible), exploring opportunities for additional tree plantings on City land, and maximizing landscaping opportunities through the Site Plan Control process.
- PR.3** The neighbourhood contains existing overhead power lines and related infrastructure, including in the front and rear yards of existing lots. Where permitted by the regulatory authority, underground relocation of this infrastructure will be encouraged through redevelopment. Where overhead power lines are maintained through redevelopment, buildings and structures will be required to adhere to any required setbacks to this infrastructure, as determined by the regulatory authority. The

implementing Zoning By-law may address compliance with building setbacks to infrastructure as a consideration for development applications.

- PR.4** Every opportunity to enhance and create both public and private parks, parkettes, walkways, and other active open spaces, will be explored to enhance public interaction. These opportunities will be created through the redevelopment of lands.
- PR.5** Improvements to pedestrian, cycling and rolling connections will be established through the redevelopment of lands to promote connectivity and provide better access.
- PR.6** The development of new buildings and the public realm will incorporate principles of Crime Prevention Through Environmental Design (CPTED), which may address such matters as: appropriate landscaping, lighting, visibility and the animation of ground floor uses. New development will also have regard for the principles of universal accessibility.
- PR.7** Improved streetscaping should be provided and may include tree lined boulevards, enhanced landscaping, paving and traffic calming, and where feasible and appropriate, through the provision of attractive street furniture and hardscape to define the public realm and neighbourhood character.

## 2.3 Structural Plan

The Structural Plan applies a layered approach that integrates land use, frontage conditions, streetscape elements and open space into a cohesive framework for a medium rise neighbourhood. This structure supports intensification while maintaining compatibility with surrounding areas, enhancing walkability, and creating a strong public realm. See **Figure 2: Structural Plan**

### 2.3.1 Land Use

The neighbourhood is organized around a medium rise residential framework that establishes a clear structure for building height and scale.

- **Minor Corridors and Major Transit Station Area:** Along Columbia Street West, Albert Street, and the north edge of Cardill Crescent (MTSA), buildings may reach up to 8 storeys in height. Concentrating taller built forms in these locations reinforces the role of the corridors as vibrant, mixed-use edges, aligns with the planned scale of the adjacent Northdale neighbourhood, and provides an appropriate transition to lower heights within the interior of Sugarbush South.
- **Interior Neighbourhood Blocks:** Within interior areas such as portions of Cardill Crescent, Hazel Street, High Street, Austin Drive, Tamarack Drive, Sorrel Place, and Smallwood Drive, buildings are generally limited to 6 storeys in height. Built forms include medium rise residential buildings, townhouses, and stacked townhouses, ensuring a finer-grain, pedestrian-oriented character. Limited non-residential uses may be included to support neighbourhood needs while maintaining a primarily residential focus.

This land use structure directs taller buildings to strategic corridors and edges, while reinforcing a more compatible, human-scaled fabric within the interior of the neighbourhood.

### 2.3.2 Street Frontages

Two primary street frontage categories define the at-grade condition:

- **Convertible Frontage:** Buildings along Columbia Street West and Albert Street are classified as Convertible Frontage that are intended to provide flexible ground floor spaces that can accommodate non-residential uses, such as retail, office, or service functions, as demand arises. These corridors function as active edges, supporting higher-intensity uses and reinforcing a vibrant main street.

- **Neighbourhood Frontage:** Buildings within the interior of the neighbourhood are classified as Neighbourhood Frontage. They provide primarily residential frontages, with the option to include smaller-scale non-residential uses. Ground-floor residential units include individual entrances directly connected to the street, creating active and engaging frontages while maintaining a residential focus. Non-residential uses are not permitted on Smallwood Drive, Sorrel Place, and Tamarack Drive.

### 2.3.3 Streetscape Elements

The existing street hierarchy of the arterial street (Columbia), collector streets (Albert, High), and local streets (Hazel, Holly, Tamarack, Cardill, Sorrel, Smallwood) is reinforced through a streetscape plan that emphasizes walkability, greenery, and well-defined building edges.

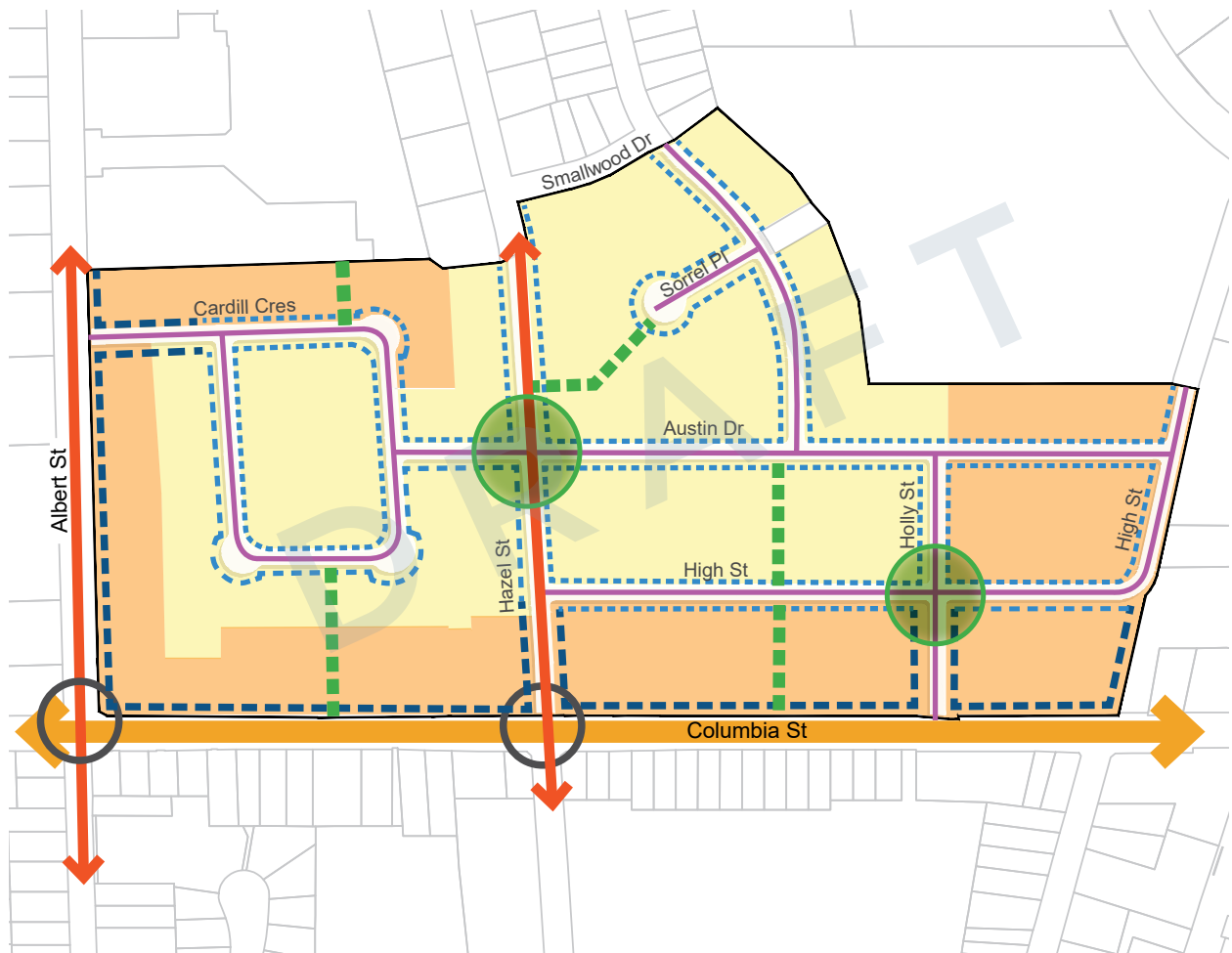
- **Columbia Street (Main Street):** Functions as the primary east-west spine with a more urban streetscape. The streetscape objective is a continuous, urban edge with pedestrian-oriented public realm with integrated active transportation and a consistent approach to planting, lighting, and street furnishings.
- **Albert Street and Hazel Street (Mixed Use Streets):** Function as important north-south connectors that link neighbourhood areas to the Main Street. The streetscape objective is to balance movement and local access and designed to foster social interaction and engagement while accommodating alternative modes of transportation.
- **Residential Streets (Holly, Tamarack, Cardill, Sorrel, Smallwood, and similar local streets):** Functions as residential streetscapes, having a more intimate scale than higher order streets such as Main Streets or Mixed Use Streets. The streetscape objective is to prioritize pedestrian comfort, neighbourhood fabric, and context-appropriate traffic calming, with landscape and lighting scaled to residential use.

In addition, a series of potential mid-block connections are identified to improve pedestrian and cyclist movement throughout the neighbourhood.

### 2.3.4 Open Space and Gateways

The plan identifies flexible sites for parkettes, generally located at high-visibility corners or where walkways converge. These public spaces would be developed through redevelopment and the specific locations would be determined through block plans. Minor gateway features will be introduced at the intersections of Columbia Street and Albert Street, and at Columbia Street and Hazel Street, to mark neighbourhood entry points, strengthen community identity, and enhance placemaking.














Land Use	Street Frontages	Streetscape Elements	Open Space and Gateways
 Medium Rise Density (8 storeys)	 Convertible Frontage (1-5m Setback)	 Main Street	 Potential Parkette Location
 Medium Rise Density (6 storeys)	 Neighbourhood Frontage (3-6m Setback)	 Mixed Use Street	 Minor Gateway
		 Residential Street	
		Potential Mid-Block Connection	

Fig.2: Structural Plan

## 2.4 Sustainable Design

Sustainability is a fundamental element of achieving a high standard of urban design in Sugarbush South and must be addressed at the earliest stages of the development process. All development should contribute to environmental performance, resilience, and long-term livability by integrating sustainability into both site planning and building design.

Buildings are expected to be compact and energy-efficient, with forms that optimize solar orientation, daylight access, and natural ventilation. High-performance building envelopes, green roofs, stormwater management systems, and renewable energy technologies should be incorporated wherever feasible. Developers are encouraged to adopt innovative and low-carbon construction techniques, such as mass timber, modular, or prefabricated systems, to reduce embodied carbon and limit construction waste.

Landscape design plays a critical role in advancing sustainability by preserving and expanding the urban tree canopy, supporting biodiversity, and improving on-site water management through sustainable stormwater practices. Public and private spaces must also be designed to promote walkability, accessibility, and year-round usability, with particular emphasis on microclimate protection and pedestrian comfort.

Sustainable design principles should be embedded early in the planning process by applicants and their consultant teams, working in collaboration with City staff. These principles must guide site planning, building design, and servicing decisions from the outset to ensure an integrated and coordinated approach. Development in Sugarbush South must demonstrate alignment with the City of Waterloo's environmental and design objectives as set out in the Official Plan and the City's Urban Design Manual. All projects must reflect a clear commitment to long-term livability, equity, and environmental performance.

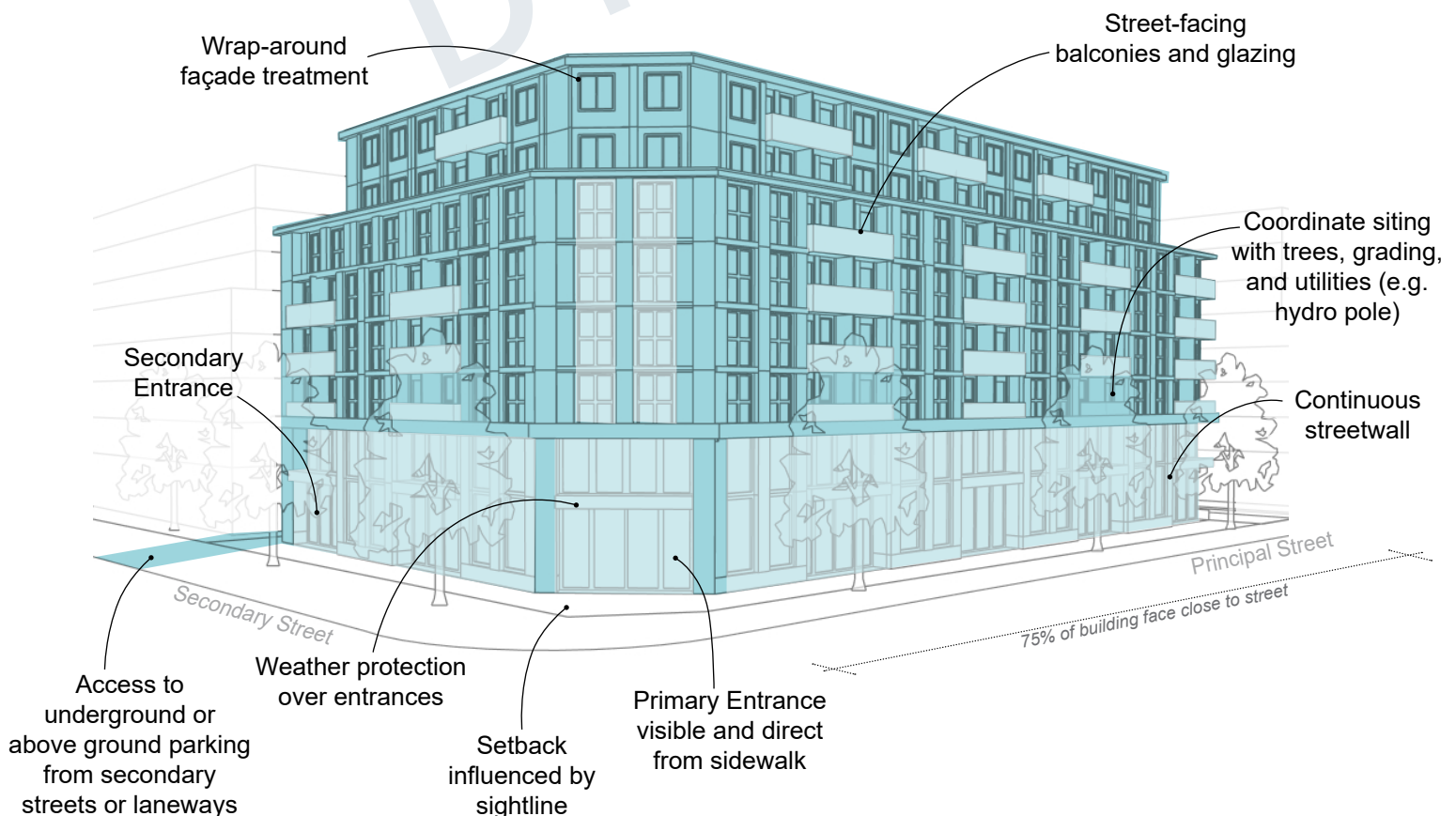
## 3.0 SITE DESIGN

### 3.1 Building Orientation and Siting

The siting and orientation of buildings shape the form and planned function of the neighbourhood. Buildings must define and animate the public realm by maintaining a strong relationship with the street. Clear entrances, active frontages, and consistent setbacks support a walkable environment and reinforce the intended role of each street. Orientation should respond to site context, enhance accessibility, and contribute to a cohesive urban fabric.

#### Guidelines:

- Buildings must be aligned parallel to the public right-of-way and oriented to frame the public realm, creating a consistent streetwall that supports a pedestrian-scaled environment.
- The primary façade shall face a public street and be placed within a consistent, context-appropriate setback that reinforces the intended



frontage type, supporting active, adaptable ground-floor space for Convertible Frontages and a greener, residential interface for Neighbourhood Frontages.

- The main entrances must be visible, legible, and directly accessible from the public sidewalk. A continuous, barrier-free pedestrian path with a minimum width of 2 metres is encouraged between the sidewalk and each entrance.
- Weather protection must be provided at building entrances through canopies, overhangs, or recesses integrated into the building architecture. These elements must be scaled to support pedestrian comfort and year-round usability.
- Parking access should be from secondary streets to reduce impacts on primary frontages.
- Buildings located on corner lots must address both streets through articulated façades and clearly defined entrances, reinforcing the importance of the intersection as a visual and functional anchor. Wrap-around façades must minimize blank walls and continue the material quality and rhythm of the primary frontage.
- Corner buildings will be subject to sightline triangles, which will influence building placement and landscape treatments.
- Buildings adjacent to parks or open spaces must front these spaces with active façades. This includes incorporating windows, balconies, and unit entrances to provide passive surveillance and activate the public edge.
- Larger building setbacks should be used strategically to avoid long, monotonous façades and to provide enhanced pedestrian amenity areas, including more space for tree planting, widened sidewalks, forecourts, plazas, or other publicly accessible open spaces.
- Building entrances must be coordinated with the location of nearby transit stops and stations to support transit accessibility and minimize walking distances for residents and visitors.
- Where hydro poles and overhead lines are present, buildings, driveways, and tree plantings must be planned to avoid conflicts. Clearances should be confirmed with the utility provider early in the design process to ensure appropriate setbacks.
- Building orientation and siting must respect existing trees, grading, utility infrastructure, noise, and engineering requirements, and be coordinated with streetscape elements to avoid conflicts with building entrances and ensure safe, accessible pedestrian circulation.



Fig.3: Recessed ground floor with an overhang provides weather protection, enhances the pedestrian experience, and creates an expanded public realm through additional setback.

## 3.2 Site Typologies

A development site's design should respond to its surrounding context through thoughtful massing, orientation, and the integration of landscaped open space. Where deeper or larger sites are created through lot consolidation, they present opportunities to introduce medium rise buildings in ways that respect adjacent low-rise areas and reinforce a cohesive urban structure.

Design strategies such as transitioning building height, locating taller elements toward the street, and ensuring adequate separation distances to adjacent uses help maintain sunlight access, privacy, and livability. These sites also play a key role in supporting walkability, internal circulation, and the overall performance of the public realm in Sugarbush South.

Further guidance on massing, scale, and transition is provided in Section 3 - Building, which outlines key built form expectations across all site types.

The following site typologies are analyzed:

**Larger Site:** A parcel created through the consolidation of multiple lots, with frontage and/or depth greater than the prevailing lot pattern, that can accommodate required setbacks, stepbacks, and separation distances; support multiple buildings or articulated wings (e.g., a meaningful courtyard or mews); organize internal pedestrian, cycling, and servicing circulation without compromising primary street frontages; and provide on-site amenity and landscape areas while stepping massing down toward sensitive residential edges.

**Deep Corner Site:** A corner parcel with greater front-to-rear depth than nearby lots, providing two public frontages and sufficient depth to accommodate setbacks/stepbacks, and landscape buffering, allowing height to focus toward the primary street and step down toward secondary street.

**Courtyard Sites (Courtyard-Form Development):** A parcel with sufficient width and depth to organize buildings around a usable interior open space while maintaining active street edges, adequate separation distances, and landscaped buffers to support light, air, privacy, and transitions. Courtyard-form development refers to building configurations (e.g., C-, H-, or U-shaped) that create and overlook a private open space.

**Through-Lot Site:** A parcel with frontage on two public streets. Design addresses both street edges with a high-quality public realm, consolidating servicing to minimise impacts on primary frontages.



**Deep Mid-Block Site:** An interior parcel with greater front-to-rear depth than nearby lots, enabling taller elements toward the primary street with stepdowns, separation, and landscape buffering toward sensitive interfaces; may accommodate internal courts or mid-block connections.

**Shallow-Depth Site:** A parcel limiting the ability to accommodate required setbacks/stepbacks and separation distances. Such parcels are better suited to 3- to 4-storey townhouse forms that can contribute to intensification while fitting within the available site geometry.

*Note: These definitions are performance-based and they guide design intent and do not supersede the Zoning By-law.*

### 3.2.1 Site Planning for Larger Sites

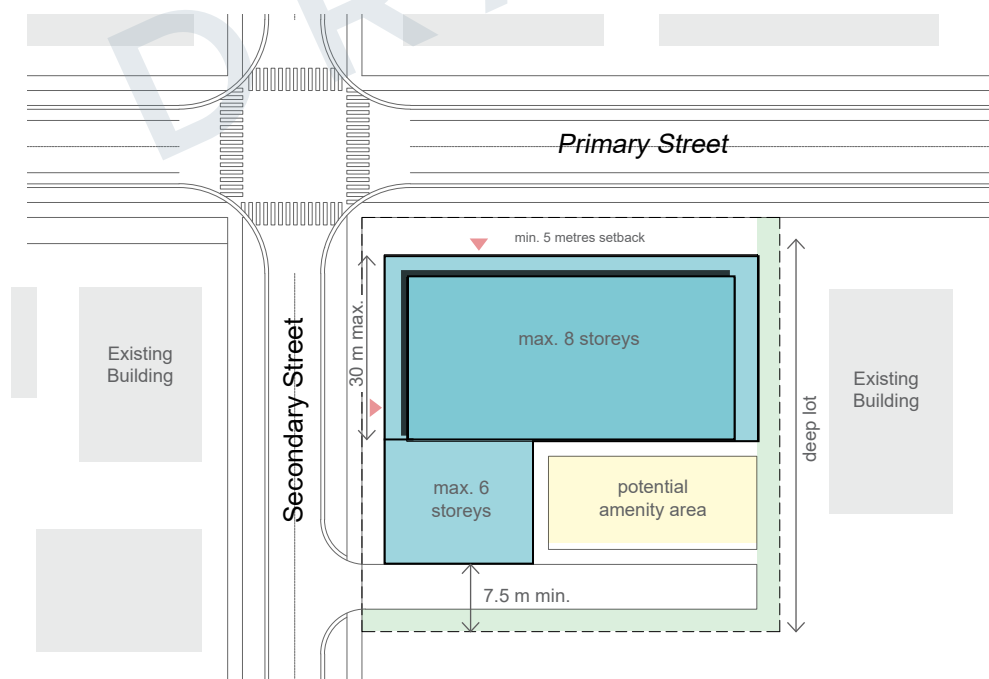
#### Guidelines:

- Buildings should be oriented to define the public street edge with active frontages and consistent massing.
- Taller building elements should be located along main street and mixed-use street, with step-downs in height toward residential street.
- A minimum separation distance of 15 metres is recommended between medium rise buildings on a singular site or on adjacent sites to ensure privacy between units and allow good access to sky view, sunlight and daylight within spaces between buildings and the units within the buildings. If a smaller distance is proposed and building should demonstrate an equivalent outcome through window offsetting or angling, upper-storey stepbacks, façade articulation to limit direct views, and landscape screening.
- Where building length exceeds 60 metres, it is recommended that the massing be broken into visually distinct volumes or multiple separate buildings to avoid the appearance of a continuous “superblock” and to reinforce a fine-grained, pedestrian-scaled rhythm along the street.
- Pedestrian walkways should be integrated through the site to support permeability and connect to the public sidewalk network.
- Loading, parking, and servicing areas should be located internally or on secondary streets to maintain the quality of the primary frontage.
- Publicly accessible open spaces, such as small plazas or walkway connections, are encouraged and should be located prominently where they can benefit both residents and the broader community.

### 3.2.2 Site Planning for Deep Corner Sites

#### Guidelines:

- Building height shall conform to the Official Plan and Zoning By-law. Concentrate greater height along Main Street and Mixed-Use Street frontages, and maintain a neighbourhood scale along Residential Streets to ensure a human-scale public realm and compatibility with adjacent dwellings.
- Where building wings extend along a Residential Street, it is recommended that building height step down from the Main or Mixed-Use frontage to the Residential Street, generally after approximately 30 metres. Where this is not feasible, demonstrate an equivalent transition through stepbacks, setbacks, or façade articulation.
- Building corners should be treated as focal points through massing articulation, distinctive architecture, public realm features, or small plazas.
- Loading, parking, and servicing areas should be accessed from the residential street and screened from both public frontages.

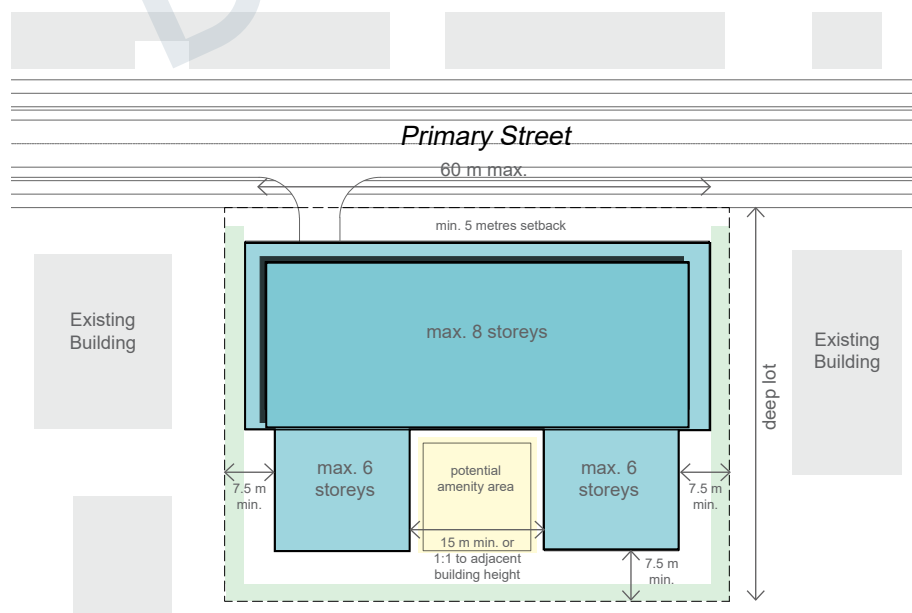




### 3.2.3 Site Planning for Courtyard Sites (Courtyard-Form Development)

#### Guidelines:

- Courtyard building configurations should only be used on sites wide enough to provide meaningful open space between building wings without compromising sunlight or privacy.
- Courtyard width should be proportionate to the average height of the facing wings, at a minimum ratio of 1:1, or 15 metres, whichever is greater.
- Courtyards should be designed as usable, landscaped amenity spaces with permeable surfaces, shade trees, and seating.
- Courtyards should not be used primarily for parking, garbage, or servicing functions.
- Building wings should be arranged to maximize daylight and views into the courtyard, avoiding direct face-to-face overlook between units.
- Courtyard-facing façades should include windows, entrances, or balconies to promote passive surveillance and residential comfort.



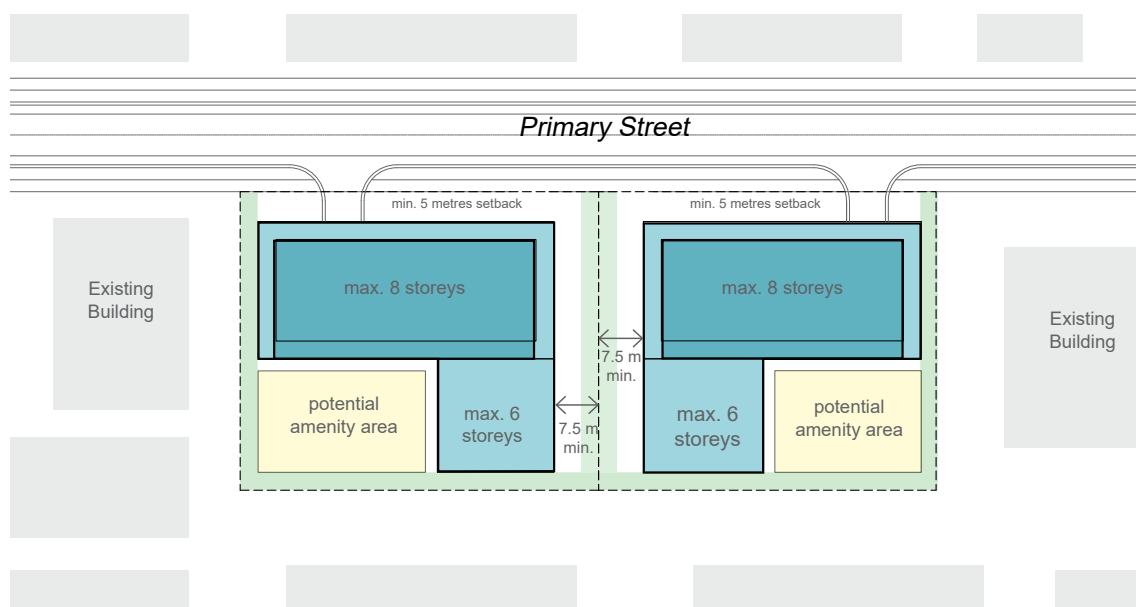
### Guidelines:

- 
- The diagram illustrates the proposed building footprint and setbacks for a development on a street corner. The street names are **Primary Street** (top) and **Secondary Street** (bottom).
- Setbacks and Buffers:**
- Primary Street (Top):** min. 5 metres setback.
  - Secondary Street (Bottom):** min. 5 metres setback.
  - Left Side (Secondary Street):** min. 3 metre landscape buffer.
  - Right Side (Primary Street):** min. 3 metre landscape buffer.
- Building Footprint and Features:**
- Left Building:**
    - taller built form
    - potential rooftop amenity area
    - Lower transition built form
  - Right Building:**
    - taller built form
    - potential amenity area
    - Lower transition built form
- Other Features:**
- Existing Buildings:** Shown in grey on the left side.
  - potential mid-block connection:** Indicated by a vertical green line between the two buildings.
  - deep lot:** Indicated by a vertical double-headed arrow on the right side.

### 3.2.5 Site Planning for Deep Mid-Block Sites

#### Guidelines:

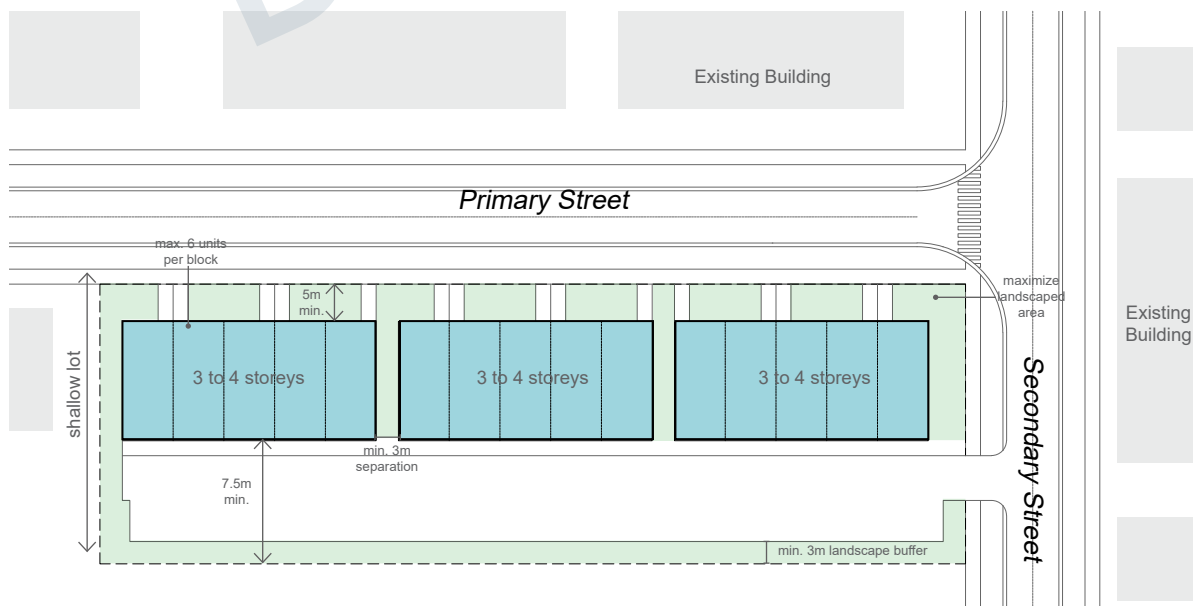
- Buildings should be oriented parallel to the street and transition in height toward adjacent lower-scale areas, with the tallest elements located along the primary frontage.
- Building wings extending perpendicular to the primary frontage should transition in scale from the main massing and be designed through stepbacks, orientation, and window placement to minimize shadow and overlook on neighbouring properties.
- Where building wings extend along a side lot line, a side yard setback of 7.5 metres is recommended to enable a 15-metre facing separation if an adjacent parcel is similarly developed. If a smaller setback is proposed, demonstrate an equivalent outcome through measures such as orienting or offsetting windows, introducing upper-storey stepbacks, articulating façades to limit direct views, and incorporating landscape screening.
- Service areas, including waste storage and loading, should be located at the rear of the site and screened from public view with landscaping or architectural treatment.
- Where adjacent sites are expected to redevelop, coordinate the alignment of access routes, open spaces, and mid-block connections to create a cohesive, walkable block structure.



### 3.2.6 Shallow-Depth Sites

#### Guidelines:

- Buildings should be oriented parallel to the street and must include primary façades that front the street with individual ground-level entrances.
- Townhouses should have a maximum height of 4 storeys. Slight differences in height are encouraged to create a varied roofline.
- Townhouse blocks (including townhouse building and freehold rows) should contain no more than 6 dwellings units. Where garages are integrated in the front of the building, individual units must be minimum 5.5 metres wide.
- Townhouses should be setback a minimum of 5 metres from the property line to accommodate front-yard landscaping. No built form elements (i.e. stairs, porches) should encroach into the first 1.5 metres of this setback.
- Landscaped front yards should be provided to support street animation, soft transitions, and tree planting opportunities.



### 3.3 Convertible Street Frontage

Convertible frontages are designed to support a range of active uses at grade, including live/work units, retail, office, or community functions. These spaces should foster vibrancy throughout the day and enhance interaction between the public realm and building interiors.

#### Guidelines:

- Ground floor units must have taller floor-to-ceiling heights, with a minimum first-storey height of 4 metres, and provide generous front glazing to ensure active and visible street frontages. Units must also be designed to allow conversion between residential and non-residential uses. A transparency level of approximately 50% of the ground floor façade glazing is encouraged along principal frontages to reinforce animation and pedestrian interest.
- Buildings should be set back a minimum of 5 metres from the front lot line to allow for porches, patios, planters, and small seating areas, while preserving the ability to transition to commercial use. A small grade change or a low fence (maximum 0.5 metres in height) may help define the transition between public and private space.
- A minimum of 75% of the street-facing façade must be located within 7 metres of the front lot line to establish a strong urban streetwall.



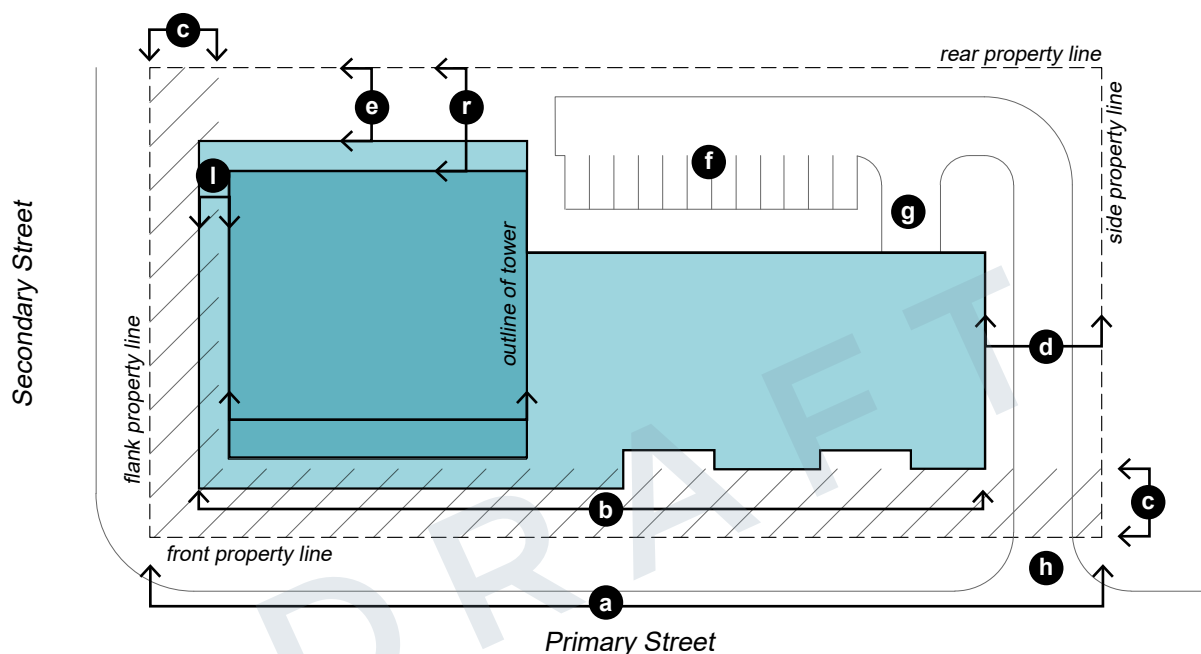
- A minimum of one first-storey entrance is required per 15 metres of street-facing building façade to promote activation, eyes-on-the-street, and a fine-grained pedestrian experience.
- Visual and physical connections between interior spaces and the public realm should be maximized through large storefront-style windows, transparent entry doors, and frequent entrances.
- Weather protection should be provided at entrances using canopies, awnings, or overhangs designed as integrated architectural elements.
- Entrances should be directly accessible from the public sidewalk via a continuous, barrier-free route with a minimum clear width of 2 metres.
- Signage should be architecturally integrated into the façade and scaled to suit the building and streetscape context.
- Utilities, vents, and service elements should not be located on prominent façade areas facing the street. Where unavoidable, they must be discreetly integrated into the architectural composition using appropriate screening or design detailing.



Fig.4: Convertible street frontage with generous glazing, seating, and pedestrian-friendly setbacks supports vibrant public-private interaction and future adaptability for a range of uses.



### 3.3.1 Convertible Street Frontage Guidance



#### A. Site

##### 1. Street Frontage

<b>a</b> Lot Frontage	20m	Minimum
<b>b</b> Building Frontage	75% - Driveway Width	
<b>c</b> Street Line Setback(s)	5 - 7m	
Setback Zone Treatment	Paved or Landscape	
Landscape Open Space	30%	Minimum

##### 2. Building Area

<b>d</b> Side Yard Setback	3m	
<b>e</b> Rear Yard Setback	7.5m	

##### 3. Parking Lot, Loading, & Access

<b>f</b> Parking Lot Location	Rear Yard and/or Side Yard	
<b>g</b> Loading Facility Location	Rear Facade	
<b>h</b> Driveway Access	Any Street (Preferably Secondary Street)	

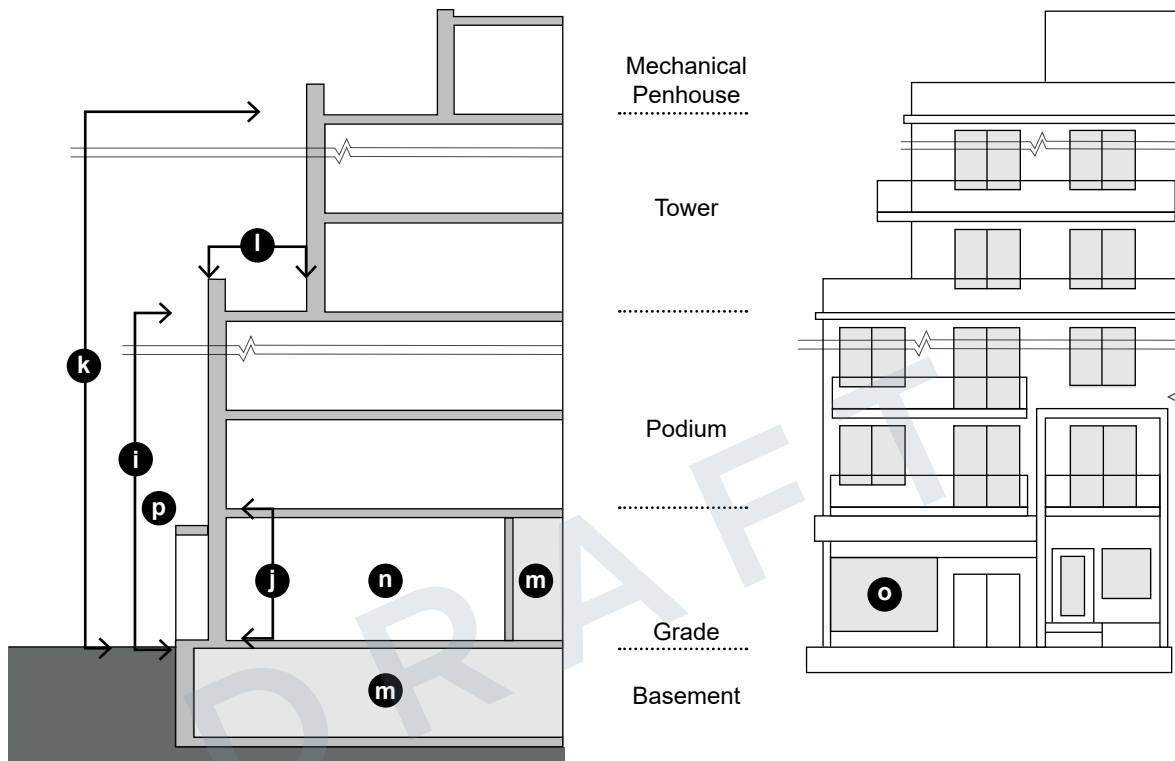
#### B. Built Form

##### 1. Podium

<b>i</b> Podium Height	3 storeys	Minimum
	6 storeys	Maximum
<b>j</b> Ground Floor Height	4m	Minimum

##### 2. Tower

<b>k</b> Total Building Height	Refer to Zoning By-law	
<b>l</b> Stepback above Podium	3m	Minimum



### C. Use

1. Grade Level		
Residential Permitted	Yes	
Non-Residential Permitted	Yes	
Parking Within Building	Permitted	
m Parking Structure Location	Rear of the ground floor habitable space	
n Habitable Area	25%	Minimum
Habitable Area Use	All uses permitted in building	
2. Upper Storeys		
Upper Storey	Refer to Zoning By-law	

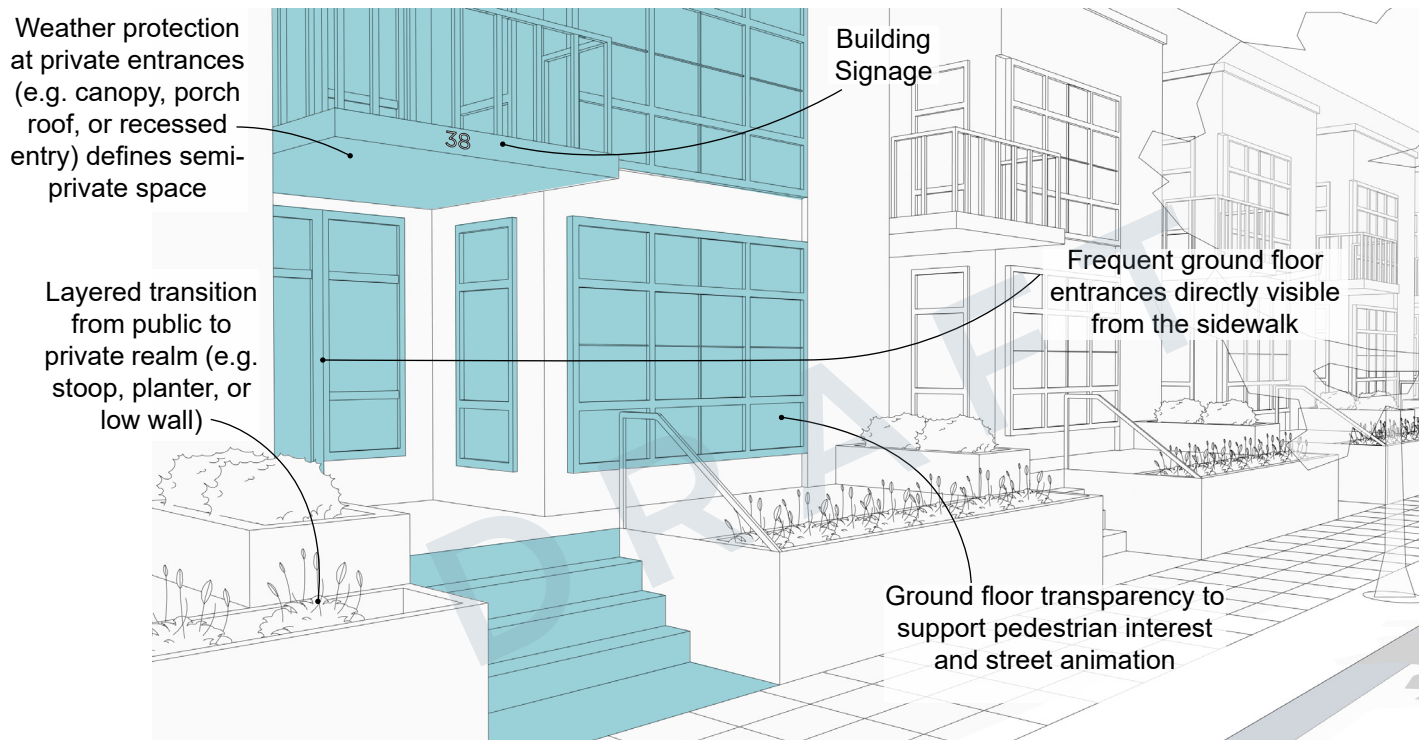
### D. Entrances & Fenestration

1. Grade Level		
o Transparency	50%	Minimum
Main Entrance Location	Front facade or corner of building	
Street Entrances	1 per 15m of facade length	Minimum
Parking Entrances	1	Maximum
2. All Levels		
p Architectural Projection	Bay window, eaves, awnings, canopies & signs	



### 3.4 Neighbourhood Street Frontage

Neighbourhood frontages support residential ground-floor units with private entrances from the street, fostering a pedestrian-friendly environment with strong street animation and passive surveillance. These frontages must balance public engagement and resident privacy through careful design of transitional spaces.



#### Guidelines:

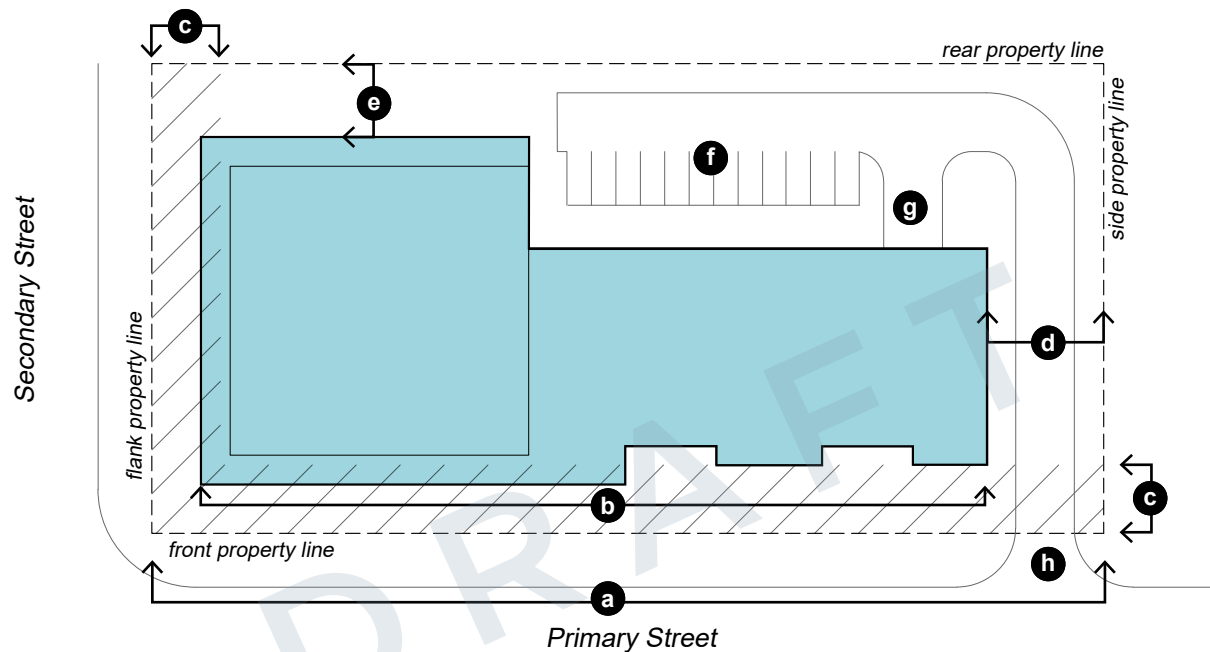
- Buildings must be setback between 5 metres and 8 metres from the front lot line, providing sufficient space for stoops, porches, planters, and soft landscaping that mark the transition from public to private space.
- Ground-floor units must include individual entrances oriented to the street, accessed via walkways that connect directly to the sidewalk. A clear barrier-free path is required.
- A minimum of one first-storey entrance is required per 15 metres of street-facing building façade or one entrance per ground-oriented dwelling unit, whichever is greater, to reinforce individual access and support residential street animation.
- The threshold between the public sidewalk and private unit entrance should be defined using a combination of elements such as low planters, elevation changes, stoops, porches, or landscaping.

- A transparency level of approximately 50% of the ground floor façade glazing is encouraged along principal frontages to reinforce animation and pedestrian interest.
- Porches should be sized to comfortably accommodate at least two people outside of the door swing. They may be enclosed or open but should retain visual and physical connection to the street.
- Landscaping should be incorporated within the front setback zone, including planting beds, small trees, or window boxes that enhance privacy and contribute to streetscape greening.
- Entrances should be illuminated by exterior lighting that enhances safety and reinforces the frontage. Lighting fixtures should be pedestrian-scaled and integrated into the architectural design.
- Weather protection should be provided at each private entrance via a porch roof, canopy, or recessed entry that also supports the definition of semi-private space.
- Utilities, vents, and other service elements should not be placed along the street-facing façade at ground level. Where necessary, these must be screened or integrated into the design without disrupting the frontage.



Fig.5: Architectural feature doubles as public seating, blurring the line between building and public realm and encouraging social interaction

### 3.4.1 Neighbourhood Street Frontage Guidance (Apartments)



#### A. Site

##### 1. Street Frontage

<b>a</b> Lot Frontage	20m	Minimum
<b>b</b> Building Frontage	75% - Driveway Width	
<b>c</b> Street Line Setback(s)	5 - 8m	
Setback Zone Treatment	Paved or Landscape	
Landscape Open Space	30%	Minimum

##### 2. Building Area

<b>d</b> Side Yard Setback	3m
<b>e</b> Rear Yard Setback	7.5m

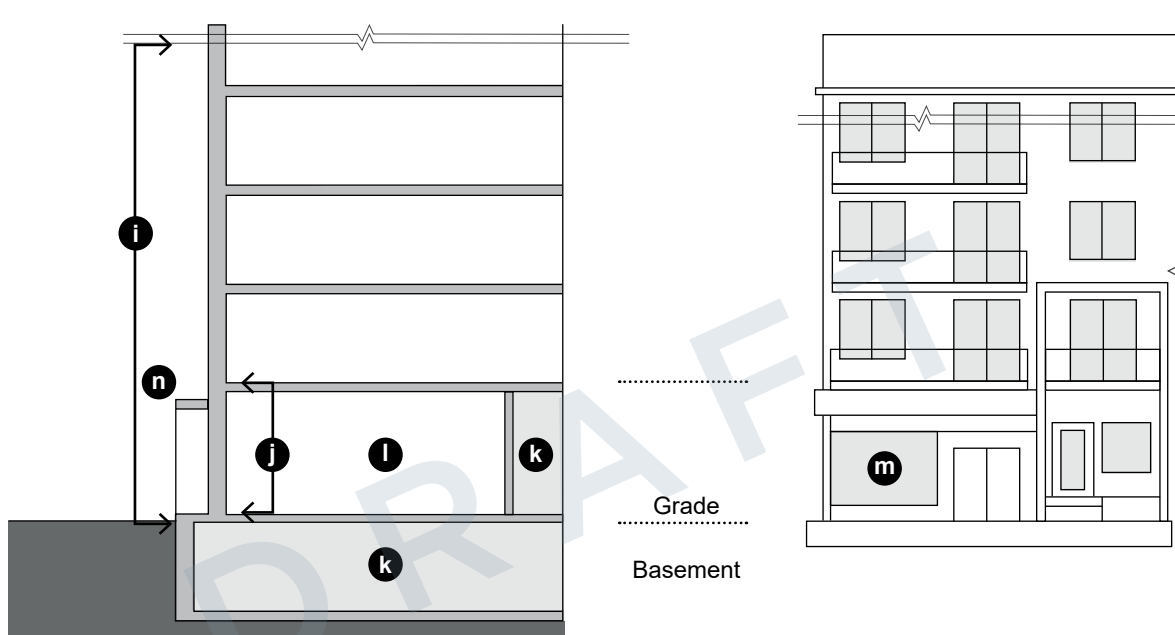
##### 3. Parking Lot, Loading, & Access

<b>f</b> Parking Lot Location	Rear Yard and/or Side Yard
<b>g</b> Loading Facility Location	Rear Facade
<b>h</b> Driveway Access	Any Street (Preferrably Secondary Street)

#### B. Built Form

##### 1. Podium

i	Building Height	Refer to Zoning By-law	
j	Ground Floor Height	4m	Minimum



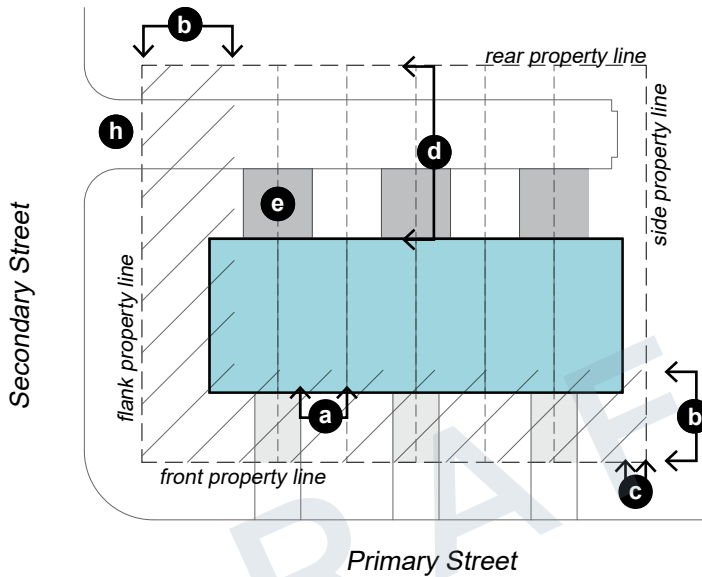
### C. Use

1. Grade Level		
Residential Permitted	Yes	
Non-Residential Permitted	No	
Parking Within Building	Permitted	
<b>k</b> Parking Structure Location	Rear of the ground floor habitable space	
<b>l</b> Habitable Area	25%	Minimum
Habitable Area Use	All uses permitted in building	

### D. Entrances & Fenestration

1. Grade Level		
m Transparency	50%	Minimum
Main Entrance Location	Front facade or corner of building	
Street Entrances	1 per 15m of facade length	Minimum
Parking Entrances	1	Maximum
2. All Levels		
n Architectural Projection	Bay window, eaves, awnings, canopies & signs	

### 3.4.2 Neighbourhood Street Frontage Guidance (Townhouse)



#### A. Site

##### 1. Street Frontage

a Lot Frontage	5.5m	Minimum
b Street Line Setback(s)	5 - 8m	
Setback Zone Treatment	Paved or Landscape	
Landscape Open Space	30%	Minimum

##### 2. Building Area

c Side Yard Setback	3m	
d Rear Yard Setback	7.5m	

##### 3. Parking Lot, Loading, & Access

e Parking Lot Location	Rear Yard and/or Side Yard (except for Freehold Townhouse)
h Driveway Access	Any Street (Preferrably Secondary Street)

#### B. Built Form

Building Height	Refer to Zoning By-law
Ground Floor Height	4m Minimum





Fig.6: Terrace planting to create sheltered entrances while maintaining the street presence.



Fig.7: A change of height to create a transition from the street and the setback provides just enough space to be useful and act as a buffer between the public and private realms.



## 3.5 Access and Parking

Access and parking should be integrated into development in a manner that minimizes visual impacts, supports pedestrian safety, and enhances the overall streetscape. The location and design of driveways and parking facilities should prioritize walkability, reduce conflicts with public realm elements, and contribute to a well-organized and functional site layout.

### 3.5.1 Vehicular Access and Driveways

Vehicular access should be carefully designed to minimize disruption to the pedestrian environment, reduce curb cuts along public streets, and support a cohesive and functional development pattern. The location, width, and design treatment of driveways should balance the needs of vehicles with the comfort, safety, and continuity of the public realm.

#### Guidelines:

- Driveway placement, width, and surface treatment should be designed to reduce conflicts between vehicles and pedestrians, while maintaining a high-quality, accessible streetscape. Architectural screening, landscape buffers, and integrated design features should be used to minimize visual disruption.
- Driveways on corner lots or sites with dual frontages should be located along the street with lower pedestrian activity, bicycle traffic, and vehicular movement to reduce conflicts and preserve active street conditions.
- Where possible, driveway entrances should align with existing street intersections or driveways on the opposite side of the street to minimize turning conflicts and improve traffic safety.
- Shared driveways and consolidated access points between multiple developments are encouraged to minimize curb cuts, preserve on-street tree planting locations, and support safer, more comfortable pedestrian environments.
- Drop-off and pick-up areas should be positioned at the side or rear of buildings and should provide a clear, direct pedestrian connection through the site to the building entrance and adjacent public sidewalk.
- Pedestrian crossings over vehicular routes should be clearly defined through changes in paving materials, textures, or colours to prioritize pedestrian visibility and safety.
- Street-facing façades along secondary frontages where driveways are located should maintain a high level of design quality, articulation, and passive surveillance to ensure that these areas contribute positively to the public realm.

- Driveway placement must be coordinated with municipal street trees and hydro pole locations. Driveway widths and spacing must allow for a continuous street tree rhythm, and designs should preserve existing trees where feasible or accommodate new tree planting with appropriate soil volumes. Where tree conflicts are unavoidable, incorporate structural soil or soil trench systems to support healthy canopy growth.
- Where possible, driveways for adjacent townhouse units should be paired to consolidate curb cuts and maximize the amount of continuous landscaped area between driveways. Paired driveways allow for larger tree planting zones, reduce hardscaping, and improve the overall streetscape quality.



Fig.8: Driveway access placed along a secondary street frontage to minimize conflicts with pedestrians, cyclists, and traffic, while preserving an active and continuous streetscape.



### 3.5.2 Parking

Parking design must support walkability, reduce visual impacts on the public realm, and integrate with the overall building and site layout. Underground and structured parking is encouraged to minimize surface parking and support compact, transit-oriented development.

#### Guidelines:

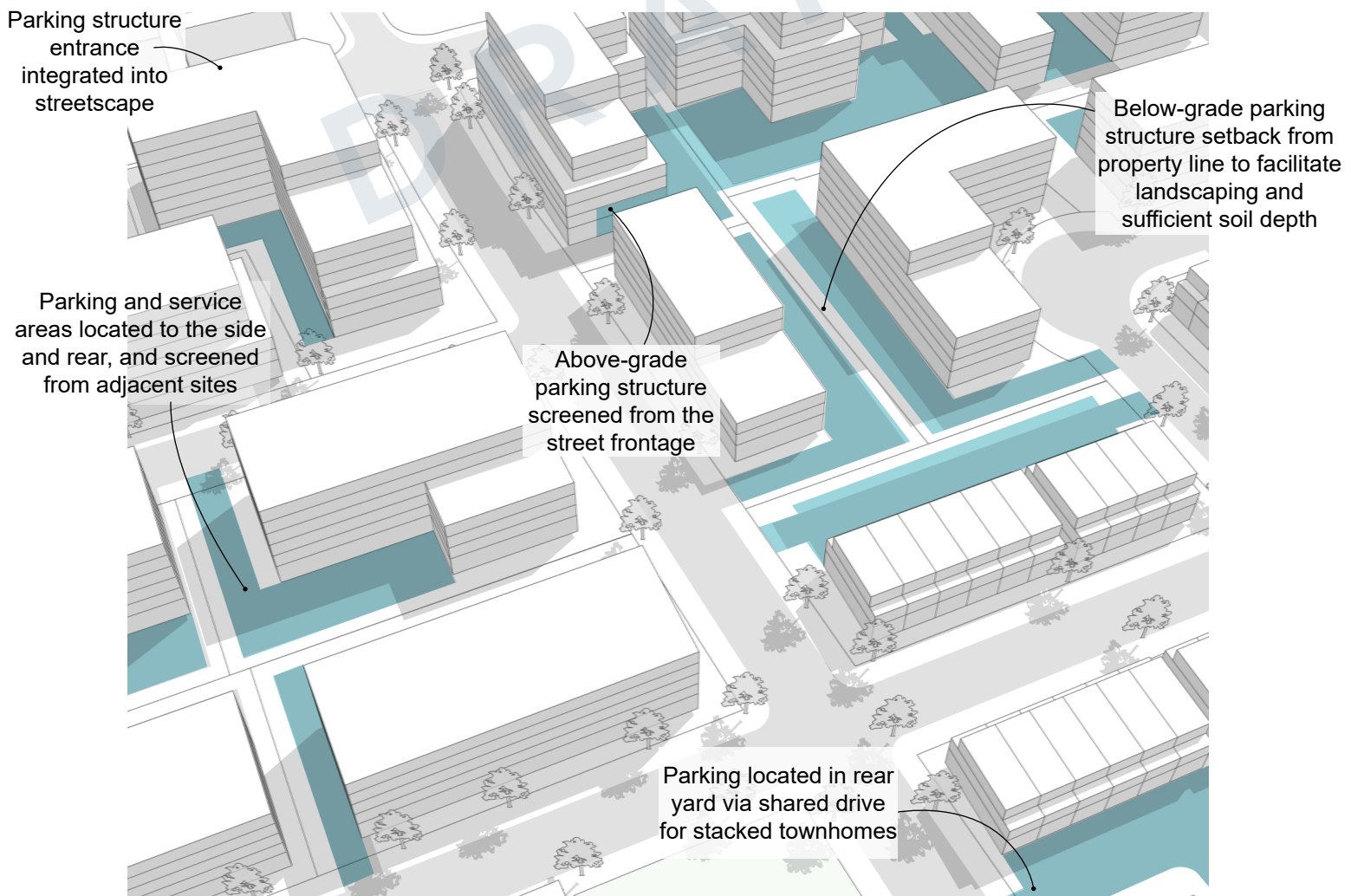
- Parking is encouraged underground or in structured parking. Surface parking must not be located at the corner of an intersection.
- Structured parking must be located behind active building uses at grade, and any portions visible from public streets must be screened with active uses, high-quality architecture, or landscaping. A minimum of 25% of the first storey must contain active or habitable uses such as residential units, circulation areas, amenity spaces, commercial or institutional uses, lobbies, or management offices that provide visual interest along the street.
- Where the first storey includes structured parking, the principal building entrance must be located on the street-facing façade. For interior lots, this refers to the front building façade. For corner lots, this refers to both the front façade and the flankage façade.
- Pedestrian access from parking areas should be clearly defined and barrier-free, with direct, safe, and well-lit pathways connecting to building entrances and public sidewalks.
- Accessible parking must be located close to building entrances, with clear signage and unobstructed access routes.
- Parking layouts should prioritize safety, with efficient circulation, appropriate turning radii, and clear sightlines.
- Where surface parking is provided, it is encouraged to incorporate low-impact development (LID) measures such as permeable paving, bioswales,



Fig.9: Parking structure entrance seamlessly integrated into the streetscape with architectural detailing and landscape elements that maintain a continuous and active public frontage.

or stormwater tree trenches to manage runoff and reduce heat island effects.

- Bicycle parking should be provided in secure, accessible, and visible locations near building entrances. Short-term bike racks should be located near primary entrances, while long-term storage should be covered or within the building footprint.
- Driveway and parking configurations should consider existing or planned street trees and should be designed to allow continuous soil volumes for healthy tree growth.
- Parking spaces should include EV-ready infrastructure to accommodate electric vehicle charging and future demand.
- Underground parking must comply with the required setbacks established in the applicable zoning category. It may, however, extend fully to the side lot line and rear lot line. Where underground parking is located beneath a required landscaped buffer, a minimum clearance of 1.2 metres must be maintained between finished grade and the underground structure.



## 3.6 Site Servicing, Waste and Utilities

Site servicing, waste management, and utility infrastructure should be carefully integrated into the overall site design to minimize visual and operational impacts, support safe and efficient functionality, and preserve the quality of the streetscape and public realm. These elements should be located and screened to reduce conflicts with pedestrian circulation, active frontages, landscaping, and open space.

### 3.6.1 Servicing and Loading

Site servicing and loading should form an integrated part of site planning, minimizing their impact on the public realm while supporting efficient operations.

#### Guidelines:

- Servicing and loading areas should be placed within building footprints or at rear/side locations, away from primary frontages.
- On-site circulation should accommodate turning movements for service vehicles without encroaching on pedestrian areas or adjacent properties.
- Pedestrian routes, amenity areas, and entrances should be clearly separated from servicing functions.
- Overhead clearance and turning dimensions must conform to UDM Part 4: Site Plan Review Guidelines under section Site Plan Standards 2D - Turning Movements.
- Service openings facing public streets should be minimized in width, recessed where possible, and designed using high-quality materials and architectural detailing.
- Shared servicing access between adjacent sites is encouraged where feasible to reduce curb cuts and maximize usable site area.

### 3.6.2 Waste Collection

Waste collection should be addressed early in the design process to ensure appropriate location, screening, and compatibility with active frontages and circulation routes.

#### Guidelines:

- Waste storage and bin staging areas should not obstruct pedestrian walkways, bicycle lanes, or tree planting zones.
- Where curbside waste collection is permitted, bins should be stored in locations that do not obstruct pedestrian, cycling, or tree planting areas and should be screened from public view. Some developments may not be eligible for municipal curbside waste collection, as eligibility is determined by the Region of Waterloo in accordance with the Waste Collection Guidelines and Waste By-law 17-007.
- If a development is ineligible for municipal collection, the property owner is responsible for arranging private waste collection. In such cases, all waste collection activities including storage, set-out, and pick-up, should occur entirely on-site. Private collection systems must meet access, clearance, and operational requirements to ensure safe and unobstructed servicing, as outlined in the Region's guidelines.
- Waste enclosures should use durable materials and be integrated into the overall site design through architectural or landscape screening.
- In multi-unit or stacked townhouse developments, shared waste storage should be consolidated and located at the side or rear of the site.



Fig.10: Discreetly integrated waste screening enhances the streetscape by concealing bins and service areas with architectural or landscape elements.



### 3.6.3 Utilities and Infrastructure

Utilities and infrastructure should be integrated into site and landscape design to reduce visual clutter and maintain a high-quality public realm.

#### Guidelines:

- Pad-mounted transformer locations must be accessible to service vehicles and must not be enclosed by fencing or obstructed by landscaping. They must be located outside the main pedestrian walkway and screened from public view.
- Where hydro poles and overhead lines are present, buildings, driveways, and tree planting must be planned to avoid conflicts. Clearances should be confirmed with the utility provider early in the design process to ensure appropriate setbacks.
- Mechanical equipment, including ventilation shafts and grilles, should not be placed within pedestrian areas or amenity areas and should be screened or integrated into the building.
- Where feasible, utilities should be buried, placed in inconspicuous locations, or screened with plantings to minimize visual impact. Any screening must maintain unencumbered access for utility providers. Utility providers are encouraged to consider innovative methods of containing utilities and determining locations for large utility equipment and cluster sites.



Fig.11: Servicing and utilities integrated into the building form to reduce visual impact and maintain a cohesive streetscape.

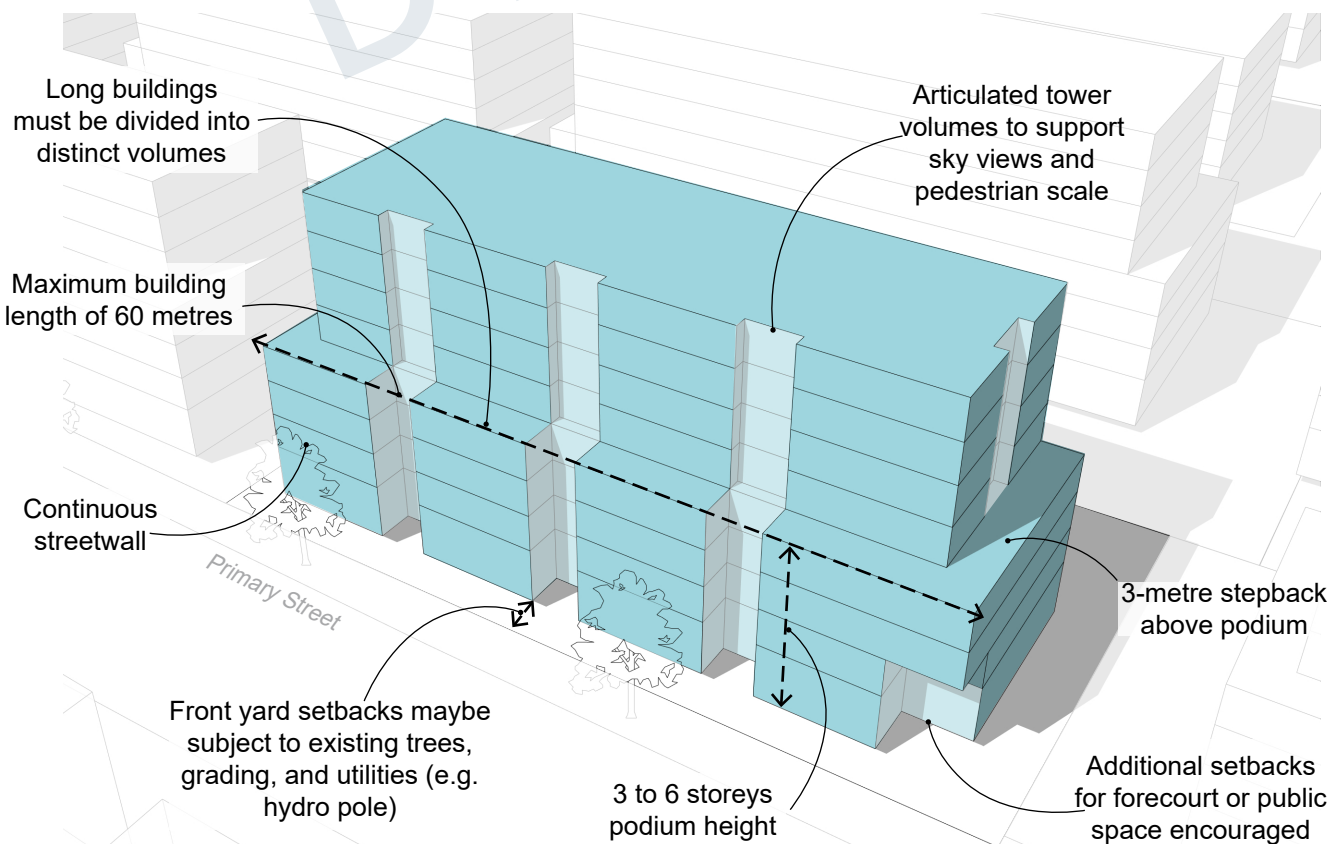
## 4.0 BUILDING DESIGN

### 4.1 Building Massing and Scale

Building mass and scale should ensure that new development fits harmoniously within its surroundings, reinforces the medium rise built form envisioned for Sugarbush South, and protects the livability of adjacent lower-density areas. Buildings should be designed to define the street edge, provide access to sunlight, support a fine-grained urban structure, and transition appropriately to adjacent properties.

#### Guidelines:

- Building heights must align with zoning provisions, generally ranging from 2 storeys for row townhouses, 3 to 4 storeys for stacked townhouses, and 6 to 8 storeys for medium rise built form. Taller buildings are permitted along Columbia Street West, Albert Street, and other designated medium rise areas, with transitions to lower-scale built form in interior blocks.
- Podium should generally be between 3 and 6 storeys and designed to define the street edge with active uses and consistent massing.



- A minimum stepback of 3 metres must be provided above the podium to reduce the appearance of building height and to support outdoor terraces or patios, where appropriate. Additional stepbacks may be required based on context, shadow, and overlook impacts.
- Buildings up to 6 storeys should have façades that are appropriately articulated, with transitions to adjacent uses achieved through setbacks and landscaping.
- Tower floorplates should be limited in width, generally not exceeding 40 metres and depth, to maintain a compact form and avoid bulky, uninterrupted massing.
- Where building length exceeds 60 metres, it is recommended that the massing be broken into visually distinct volumes or multiple separate buildings to avoid the appearance of a continuous “superblock” and to reinforce a fine-grained, pedestrian-scaled rhythm along the street.
- Townhouse blocks should contain no more than 6 dwellings units. Where garages are integrated in the front of the building, individual units must be minimum 5.5 metres wide.
- Balconies should be integrated within the building mass and not project over sidewalks or the public right-of-way. Recessed or inset balconies are encouraged to minimize shadow and overlook impacts.
- The building should provide articulated façades with a high degree of transparency and street animation, including windows and entrances along all public frontages. Approximately 50% transparency is encouraged along ground-floor frontages to promote visibility, accessibility, and pedestrian interest.
- Building massing at corner sites should wrap both frontages, emphasize the corner condition or distinctive architectural treatment to reinforce the site’s prominence, support wayfinding, and contribute to placemaking.
- Building design should respond to site constraints such as topography, existing tree canopy, and utility easements by integrating entrances, landings, and landscaping that accommodate grade changes and preserve key site features. The vertical distance between the sidewalk and building entrance should not exceed 1.5 metres.





Fig.12: Articulated stacked townhouse massing with stepped volumes, recessed balconies, and varied heights to reduce visual bulk. The overall scale aligns with the street, while raised entries and landscape buffers provide a defined transition from public sidewalk to private units.



Fig.13: Medium rise building with a well-defined podium, upper-storey stepback, and articulated facades that reinforce pedestrian scale, corner prominence, and compatibility with surrounding context.





Fig.14: Corner building with articulated massing, active frontages on both streets, and consistent streetwall height that reinforces pedestrian scale and enhances the intersection's identity.



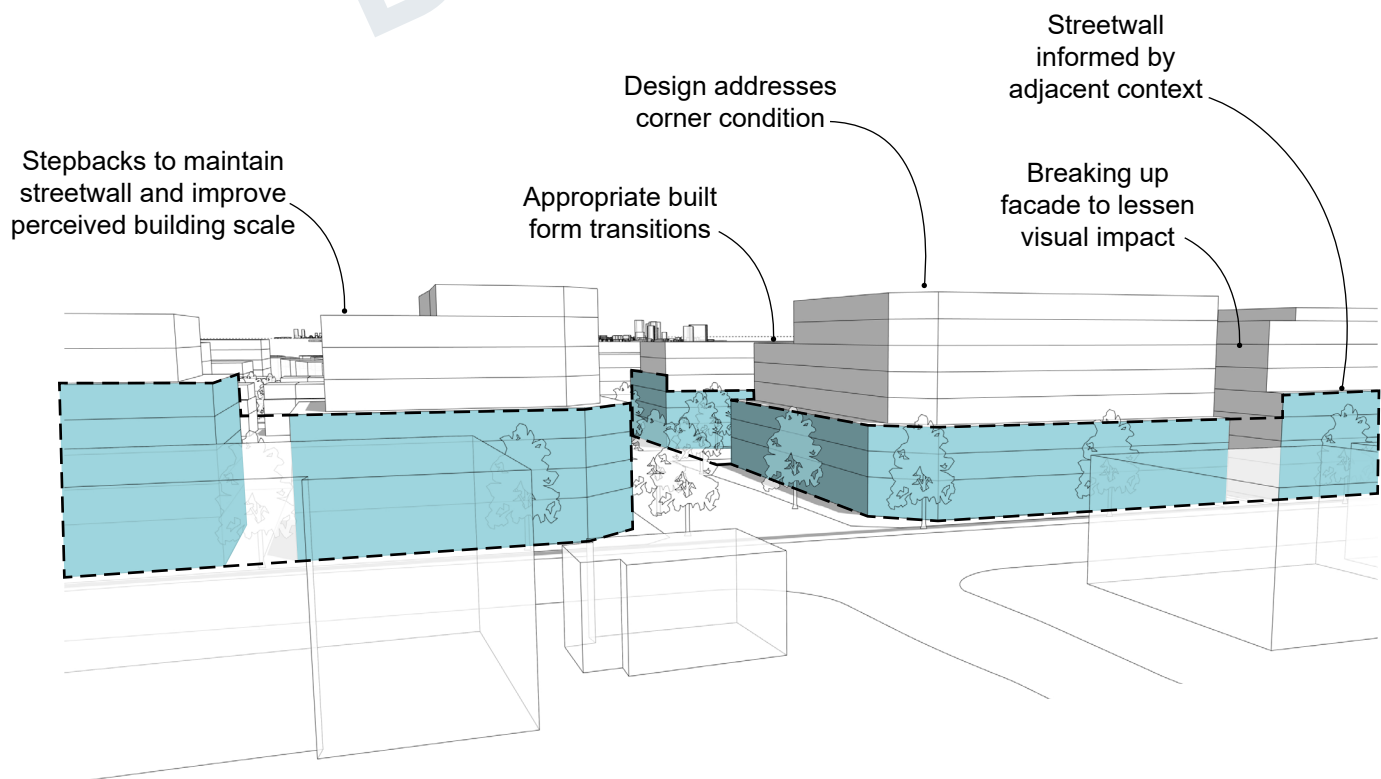
Fig.15: Medium rise building with a transparent, double-height base that animates the public realm, articulated upper storeys with a stepback to reduce visual bulk, and a clear material change that distinguishes the base from the upper levels and reinforces a human-scaled streetwall.

## 4.2 Streetwall Design

Streetwalls are generally defined by a series of continuous building facades that are typically parallel to a public realm. Buildings should frame and support the public realm through a consistent, well-articulated streetwall that reinforces pedestrian scale, walkability, and visual rhythm, to animate and define these frontages.

### Guidelines:

- A consistent architectural rhythm and human scale should be established through well-proportioned façades that incorporate material changes, window groupings, and vertical and horizontal articulation.
- Where a consistent streetwall pattern or architectural rhythm exists, new development should align with adjacent frontages and reflect common elements such as vertical breaks, window proportions, and horizontal features like trim or rooflines.
- Where no prevailing pattern exists, establish a new streetwall rhythm that supports pedestrian scale through façade articulation, window spacing, and massing modulation.
- Where building length exceeds 60 metres, it is recommended that the façades should be broken up through vertical articulation and material changes to avoid monotony. Where appropriate, incorporate recessed bays





or step-backs at intervals to soften perceived building scale and provide transition to adjacent uses.

- A storefront condition along convertible frontages should include recessed entries, transom windows, bulkheads, and signage bands to reflect a traditional retail format.
- Transparency between indoor and outdoor spaces should be maximized with frequent, well-distributed windows and entrances along all street line (public-facing) building façades.
- Corner sites should reflect their prominence through distinctive massing, enhanced architectural detailing, or the inclusion of small public spaces such as forecourts or plazas that support wayfinding and placemaking.
- Greater setbacks may be provided at strategic locations to support tree planting, sidewalk widening, and enhanced pedestrian amenities.



Fig.16: Medium rise buildings that reinforce a pedestrian scaled streetwall.

## 4.3 Transition to Buildings

Development should respond sensitively to adjacent properties through thoughtful massing, spatial separation, and landscaping. Transitions between buildings need to take into consideration the planned function of the area and the surrounding context.

### Guidelines:

- Transition in height should be achieved through a combination of setbacks, stepbacks, massing variation, and landscape buffers to minimize impacts on adjacent lower-scale properties.
- A minimum rear yard setback of 7.5 metres is required from the rear property line to the building face.
- A minimum separation distance of 15 metres is recommended between medium rise buildings on a singular site or on adjacent sites to ensure privacy between units and allow good access to sky view, sunlight and daylight within spaces between buildings and the units within the buildings. If a smaller distance is proposed and building demonstrate an equivalent outcome through window offsetting or angling, upper-storey stepbacks, façade articulation to limit direct views, and landscape screening.
- A landscape buffer of minimum 3 metres should be integrated along rear and side edges, wherever possible, to soften transitions and support new tree planting. Retain mature trees along shared property lines where feasible, and provide appropriate soil volumes to support healthy growth.
- Public realm transitions should be framed through consistent streetwall heights (typically 3 to 6 storeys) before upper storeys are stepped back. Stepbacks should reinforce pedestrian scale and reduce the visual impact of taller buildings from the street.





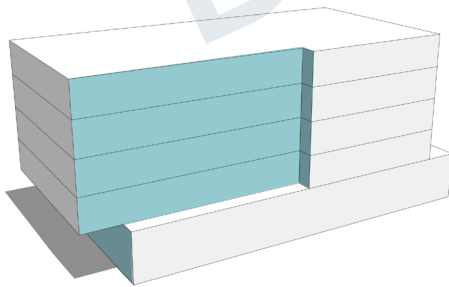
Fig.17: **Top image** – Rear and secondary street frontage: This view illustrates how stepbacks and massing articulation are applied on the back side of the building to reduce scale and overlook impacts on the adjacent low-rise context. **Bottom image** – Primary street frontage: This view shows the building's main façade along the primary street, where the 8-storey streetwall maintains a strong pedestrian scale. Upper level steps back to reduce shadow and visual impacts, while the façade is animated through material variation, extensive glazing, and active ground-floor uses that reinforce a vibrant and human-scaled streetscape.

## 4.4 Façade Design and Articulation

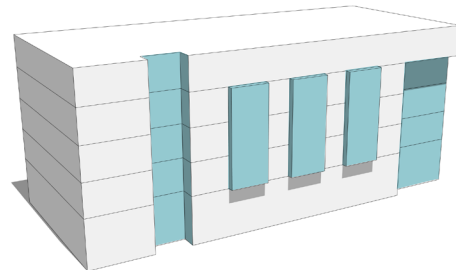
Building façades must be designed to support pedestrian scale, reinforce architectural rhythm, and contribute to a visually interesting and cohesive streetscape.

### Guidelines:

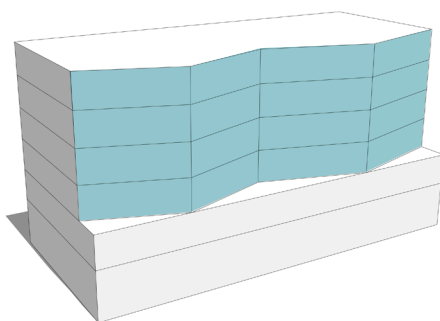
- Facades should be organized with podium and tower (if applicable), using consistent materials, proportions, and detailing to support a cohesive architectural expression.
- A high level of articulation must be provided at the ground floor to support a human-scaled public realm. Techniques may include recessed entrances, window bays, canopies, or fine-grained material changes.
- Articulation should distinguish the lower storeys from upper levels through features such as consistent cornice lines, material transitions, or step-backs, reinforcing a sense of scale and rhythm along the street.
- Corner sites, including gateway lots at key intersections or view termini, must be designed with equal façade treatment and massing quality on both frontages. Enhanced architectural expressions such as distinctive massing, rooflines, or prominent entrance should reinforce their prominence and contribute to neighbourhood identity.



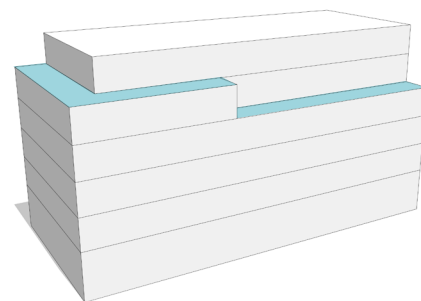
Horizontal Shifts



Vertical Shifts



Angular Shifts



Upper storey Stepbacks





Fig.18: Contemporary medium rise form with consistent window rhythm, recessed balconies, and high ground-floor transparency supporting a vibrant and pedestrian-friendly streetscape.



Fig.19: Long facades are broken down using elements such as bays, balconies and terraces.

- Long façades should be broken up through vertical articulation, recessed bays, or height variation to avoid visual monotony and reinforce the underlying lot rhythm.
- Balconies should be recessed or integrated within the building mass to minimize shadow, overlook, and wind impacts. Projections into the public right-of-way are discouraged.
- Blank walls should be avoided. All visible elevations must incorporate a level of design consistent with the primary frontage and respond to adjacent conditions.
- On larger sites, architectural variation is encouraged between buildings or blocks to avoid sameness. However, a consistent design language should be maintained to reinforce visual cohesion and a unified sense of place.
- Development should utilize long lasting, high quality building materials (brick, stone, and wood) chosen for their functional and aesthetic qualities, compatibility with surroundings and energy and efficiency
- Mechanical equipment must be screened from view and should not be located within balconies or facing public frontages.

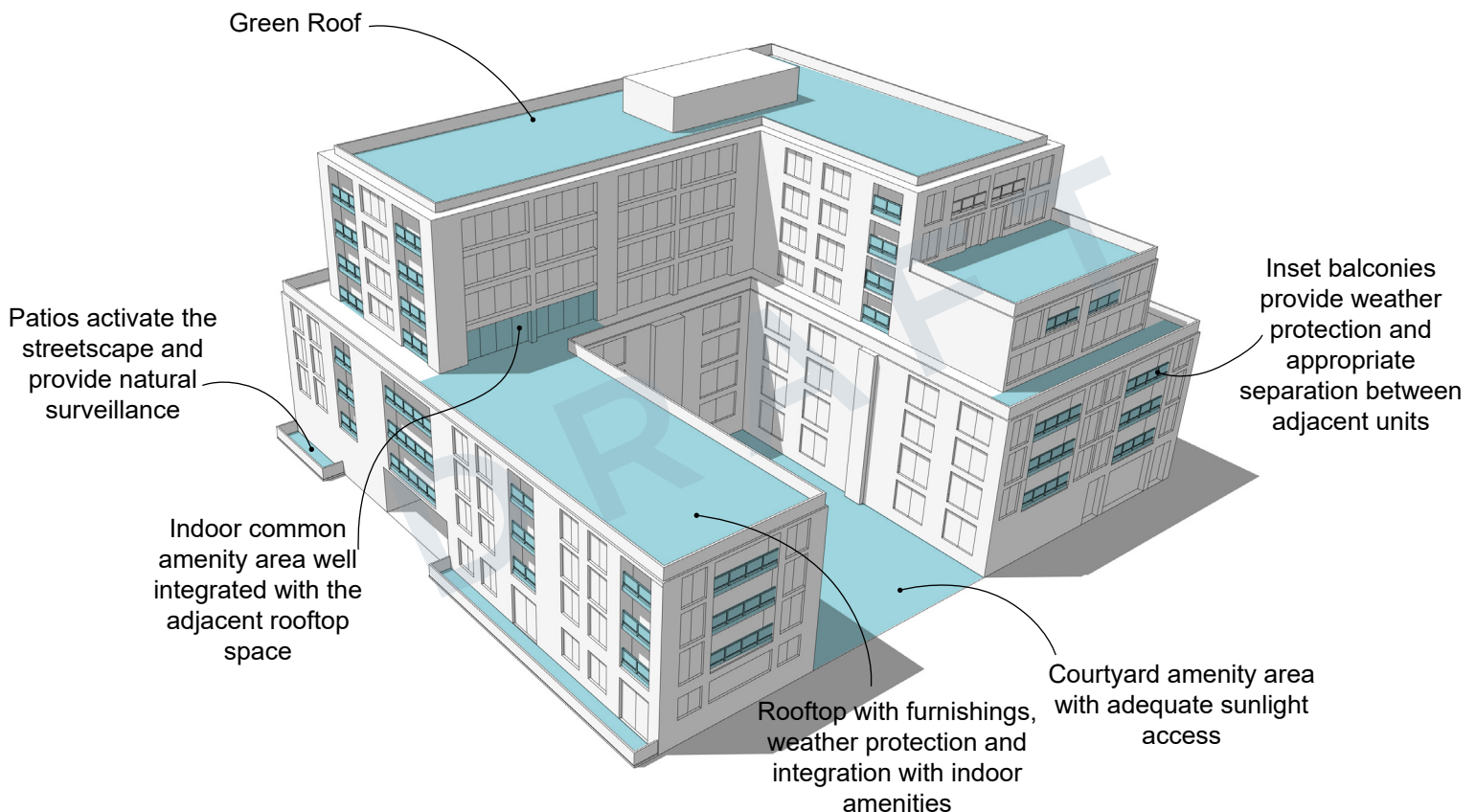


Fig.20: Medium rise building with distinct material transitions and a recessed top storey, complemented by a raised landscaped buffer and integrated ramp that support accessibility and define a clear transition between the public realm and private entrances.



## 4.5 Private and Common Amenity Areas

All developments should provide a mix of indoor and outdoor amenity areas that support the health, comfort, and social needs of residents. Amenity spaces should be designed to encourage year-round use, promote accessibility, and contribute to the overall livability and quality of the site.



### 4.5.1 Common Amenity Areas

#### Guidelines:

- A minimum amount of shared amenity space should be provided for all residential or mixed-use buildings in accordance with zoning requirements.
- Outdoor common amenity areas should be located to receive good sunlight, avoid prevailing winds, and be buffered from noise and service areas.
- Where possible, outdoor common spaces should be provided at grade to enhance accessibility, visibility, and integration with the public realm.

- Amenity areas should include durable and attractive features such as seating, shade structures, trees, pedestrian-scale lighting, and four-season landscaping.
- Where provided, rooftop amenity spaces should be designed with protective screening, wind mitigation, and bird-friendly features.
- Indoor amenity rooms should be located adjacent to outdoor amenity areas and visually connected through windows or direct access points.
- Shared amenity areas should be designed to serve a range of age groups and household types, including families with children, youth, and seniors.
- Where appropriate, pet-friendly features such as small dog relief areas or pet wash stations should be included.
- Common Outdoor Amenity Spaces should be located away from building servicing, parking and loading functions. If this is not possible, servicing areas should be heavily screened with a landscape buffer and a fence.
- All common outdoor amenity areas must have barrier-free access to the building and the public right-of-way. Site furnishings must comply with AODA standards.
- Microclimatic conditions should be considered in the design of all outdoor amenity spaces, including the placement of vegetation, shade elements, and wind protection to provide a balanced mix of sun and shelter.

#### **4.5.2 Private Amenity Areas (e.g., Balconies, Rooftop etc.)**

##### **Guidelines:**

- Private outdoor space should be provided for each dwelling unit in the form of balconies, terraces, patios, or roof decks.
- Balconies should be designed to accommodate outdoor furniture and allow for meaningful use (e.g., space for seating or planting).
- Balconies should be recessed or partially inset into the building mass to improve comfort, minimize wind, and enhance privacy.
- Private amenity areas at grade, such as patios or garden spaces, should include landscaping and visual screening to reduce overlook from adjacent sites or public areas.
- Rooftop terraces for private use must be designed to limit visual and acoustic impacts on adjacent properties and integrate screening or planting where appropriate.





Fig.21: An internal courtyard designed as a shared outdoor amenity space, offering a mix of seating, greenery, and shaded areas that promote relaxation, social interaction, and a strong sense of community within a residential development.



Fig.22: Rooftop terrace with seating, BBQs, and plantings offers residents a shared space for gathering, relaxing, and enjoying city views.

## 4.6 Rooftop Mechanical and Mechanical Systems

Mechanical systems should be integrated into building and site design in a manner that minimizes visual and acoustic impacts, protects the quality of the public realm, and ensures compatibility with adjacent uses. This includes rooftop mechanical equipment, vents, utility boxes, and other service elements.

### Guidelines:

- Rooftop mechanical equipment should be fully screened from public view using parapets, architectural screening, or enclosures that are consistent with the building's materials and colour palette.
- Mechanical screening should be incorporated into the overall massing of the building and designed as an integrated architectural feature.
- Any rooftop access hatches, guardrails, or safety apparatus must be unobtrusive and located to reduce their visibility from surrounding streets and properties.
- Mechanical systems for individual units, such as air conditioners, should not be placed on porches and balconies where it will impact the usability of these spaces.

For additional guidance on ground-level utility infrastructure and mechanical elements, refer to Section 3.6.3 Utilities and Infrastructure.



## 5.0 PUBLIC REALM

### 5.1 Streetscape Design

Streetscapes in Sugarbush South should be tailored to the intended function, activity level, with coordinated elements that reinforce street hierarchy and promote walkable, human-scaled environments. Three street typologies, Main Street, Mixed Use Street, and Residential Street, are identified in Official Plan Schedule 'A45a' - Street Frontage Areas and Streetscape Elements, Official Plan Schedule 'E' - Road Classification System outlines the road classifications (e.g., primary or secondary roads).

All streetscapes must be designed in accordance with the City of Waterloo's standards, including the Urban Design Manual, Accessibility Standards for the Design of Public Spaces, Comprehensive Engineering and Landscape Manual and Transportation Master Plan. Coordination with Enova Power and other utility providers is required to ensure that above- and below-grade infrastructure does not impede sidewalk alignment, tree planting, or overall public realm performance.



Fig.23: A well-designed streetscape with wide sidewalks, street trees, and active frontages enhances pedestrian comfort, supports walkability, and creates an inviting public realm.

### 5.1.1 Main Street

- Columbia Street West is classified as Main Street and is intended to support high pedestrian volumes and multi-modal activity.
- Sidewalks should be a minimum of 2.1 metres in width to support pedestrian flow and café seating, with a 2.0-metre boulevard furnishing zone for street trees, lighting, bike racks, and seating.
- Boulevards should accommodate street trees in connected tree trenches and include permeable paving or green infrastructure where feasible.
- Pedestrian-scaled lighting, benches, and bike racks should be provided at regular intervals.
- Medians or curb extensions may be introduced where feasible to support crossing safety and greening.
- Streetscape elements should be coordinated with utilities to prioritize tree planting and pedestrian comfort.
- Driveways and curb cuts should be minimized to preserve active frontage and public realm continuity.

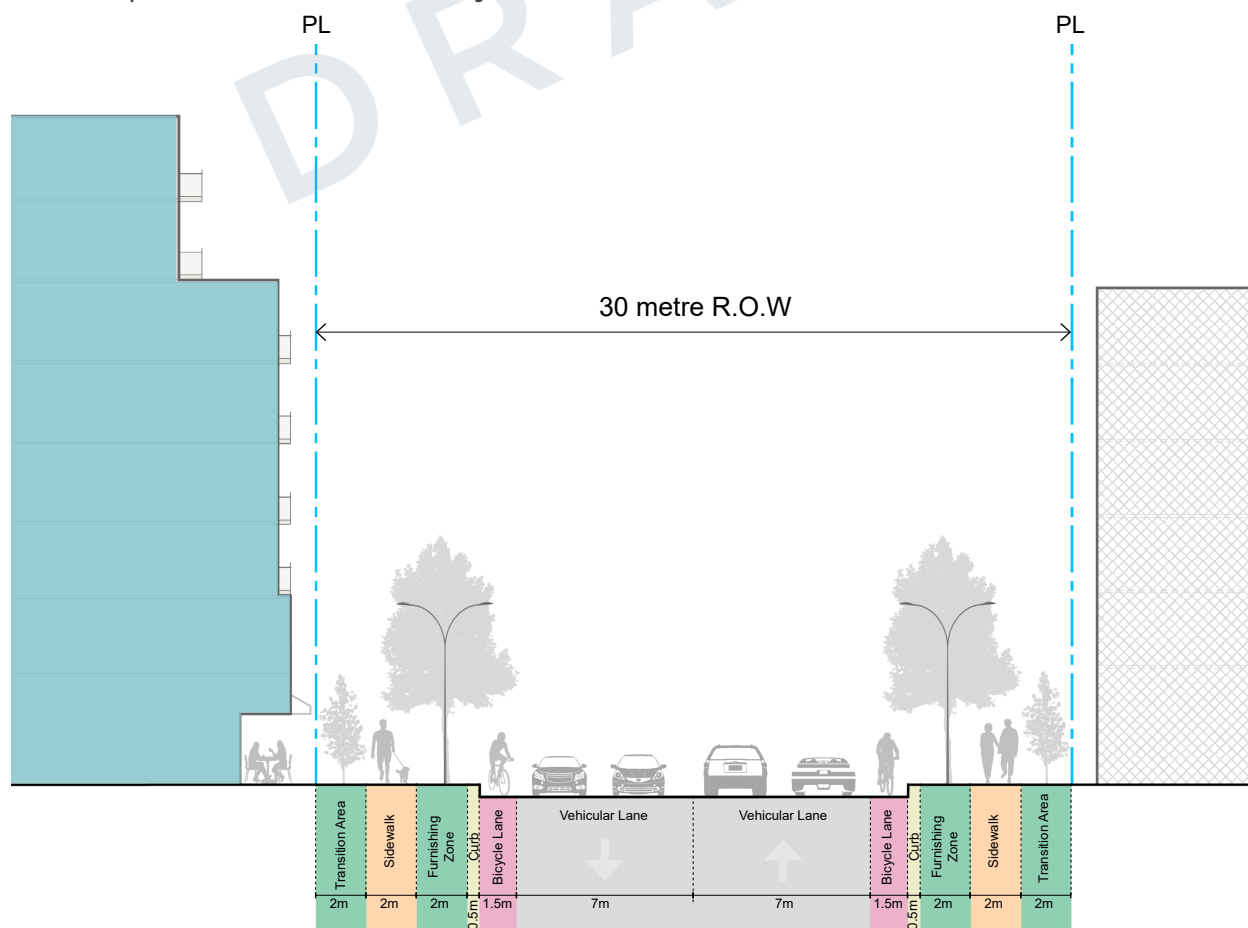


Fig.24: City Arterial Road



## 5.1.2 Mixed-Use Street

Albert Street and Hazel Street are classified as Mixed-Use Streets, that provide a flexible and animated streetscape environment that are designed to accommodate ground-floor commercial uses. These streets foster social interaction, activation, and safe mobility.

- A minimum 2-metre boulevard sidewalk zone should be provided, along with a 2-metre furnishing zone that accommodates seating, bike parking, tree planting, and street furniture.
- Curb extensions, permeable pavers, and stormwater-capturing tree pits should be considered to enhance environmental performance.
- Mixed-use street should incorporate flexible parking arrangements with materials that distinguish the pedestrian realm from vehicular areas.
- Street furnishings such as bike racks, benches, waste receptacles, and lighting should be provided to support walkability.
- Trees should be planted in connected trenches or enhanced soil zones with spacing that supports a continuous canopy.
- Driveways and curb cuts should be minimized to preserve active frontage and public realm continuity.

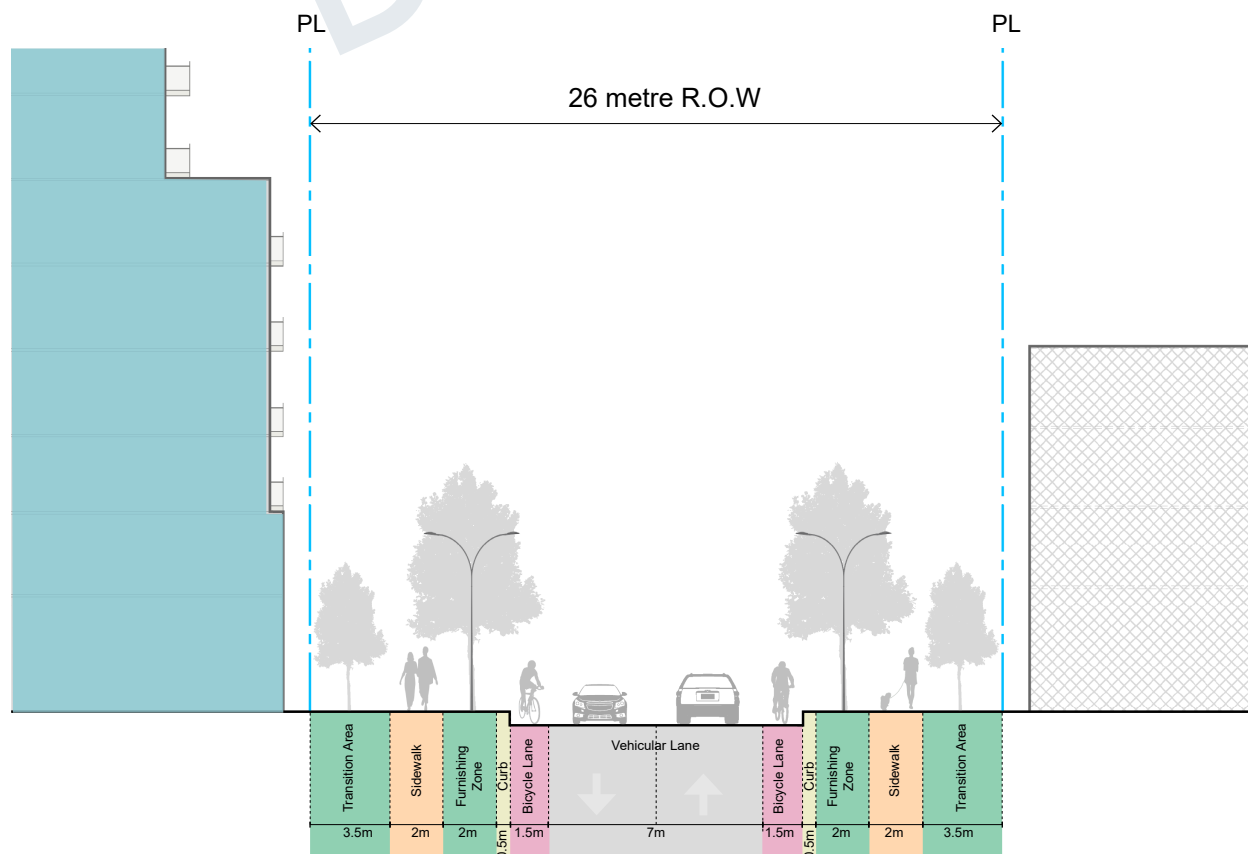


Fig.25: Major Collector Road

### 5.1.3 Residential Streets

Residential Streets form the internal local road network and must foster a calm, walkable, and green streetscape with enhanced tree canopy and front yard landscaping.

- Sidewalks must be a minimum of 1.8 metres wide, with a landscaped boulevard planted with street trees at regular intervals.
- A continuous planting zone of at least 1.5 metres must be provided between the sidewalk and curb, with street trees spaced at 6 to 8 metres intervals.
- Front yard landscaping should reinforce public/private transitions and support tree canopy growth.
- Pedestrian-oriented lighting and front porches are encouraged to support safety and community interaction.
- Utility infrastructure (e.g., hydro poles, transformers) should be sited to avoid conflicts with tree planting or sidewalk alignment.

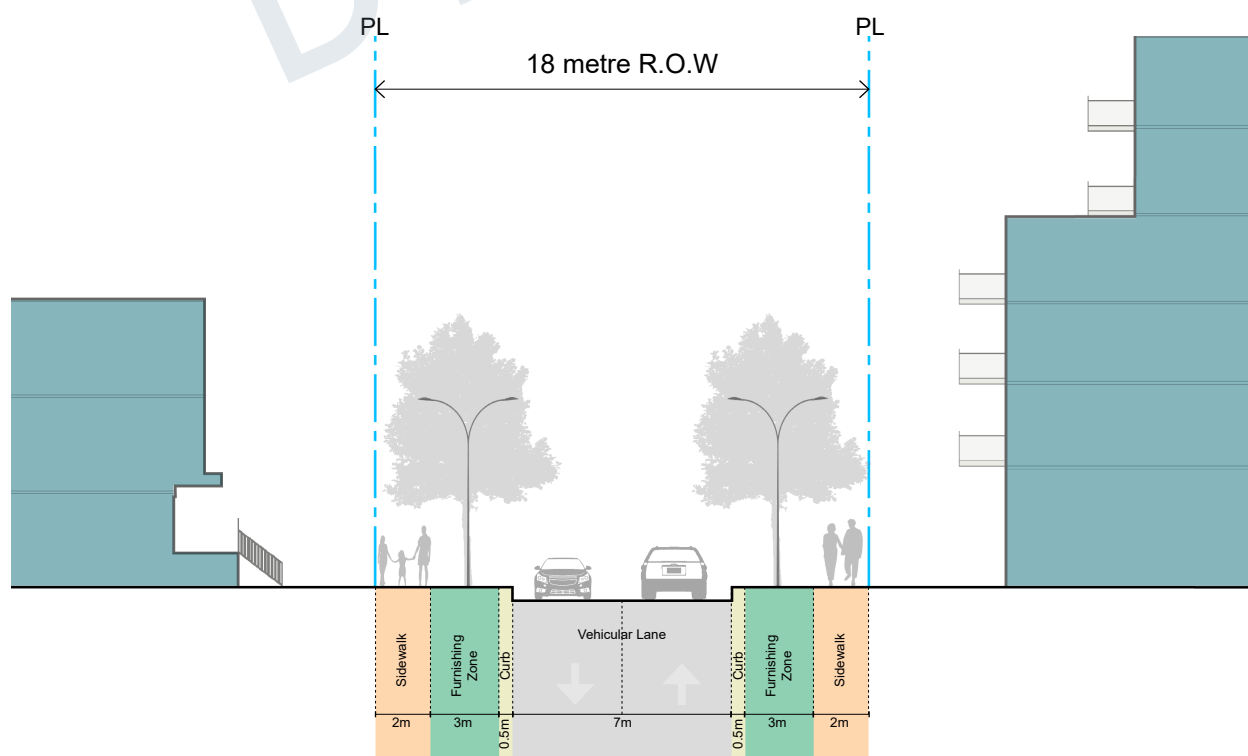


Fig.26: Local Road

Mid-block connections improve walkability by providing safe, direct access through large blocks and facilitating a more connected neighbourhood. Mid-block connections (or pedestrian corridor) are identified in Official Plan Schedule 'F' - Active Transportation Framework and will be secured through the development approval process where applicable.

- Where required, mid-block connections will be secured through land conveyance, unless determined otherwise by the City to ensure long-term public access.
- A minimum walkway width of 3 metres must be maintained, in accordance with City standard CW-502 as set out in the Comprehensive Engineering and Landscape Manual.
- Mid-block connections must be designed to maintain clear sightlines, unobstructed access, and barrier-free movement throughout.
- Direct pedestrian access must be provided between the mid-block connection and the adjacent public street.



- Where mid-block connections meet a street or open space, turning building frontages around corners (with corresponding civic space) to create inviting entrances.
- Lighting, signage, and street furniture should be incorporated to improve comfort and safety, particularly for evening use.
- Connections should include a combination of hard and soft landscaping, low plantings, and street trees to enhance comfort and reinforce the transition between public and private spaces.
- Utilities, waste collection, and building equipment should be located away from pedestrian walkways to minimize visual clutter and reduce impacts on comfort and safety for pedestrians and cyclists.



Fig.27: Mid-block connection with integrated public art, planting, and lighting to enhance safety, identity, and visual interest.



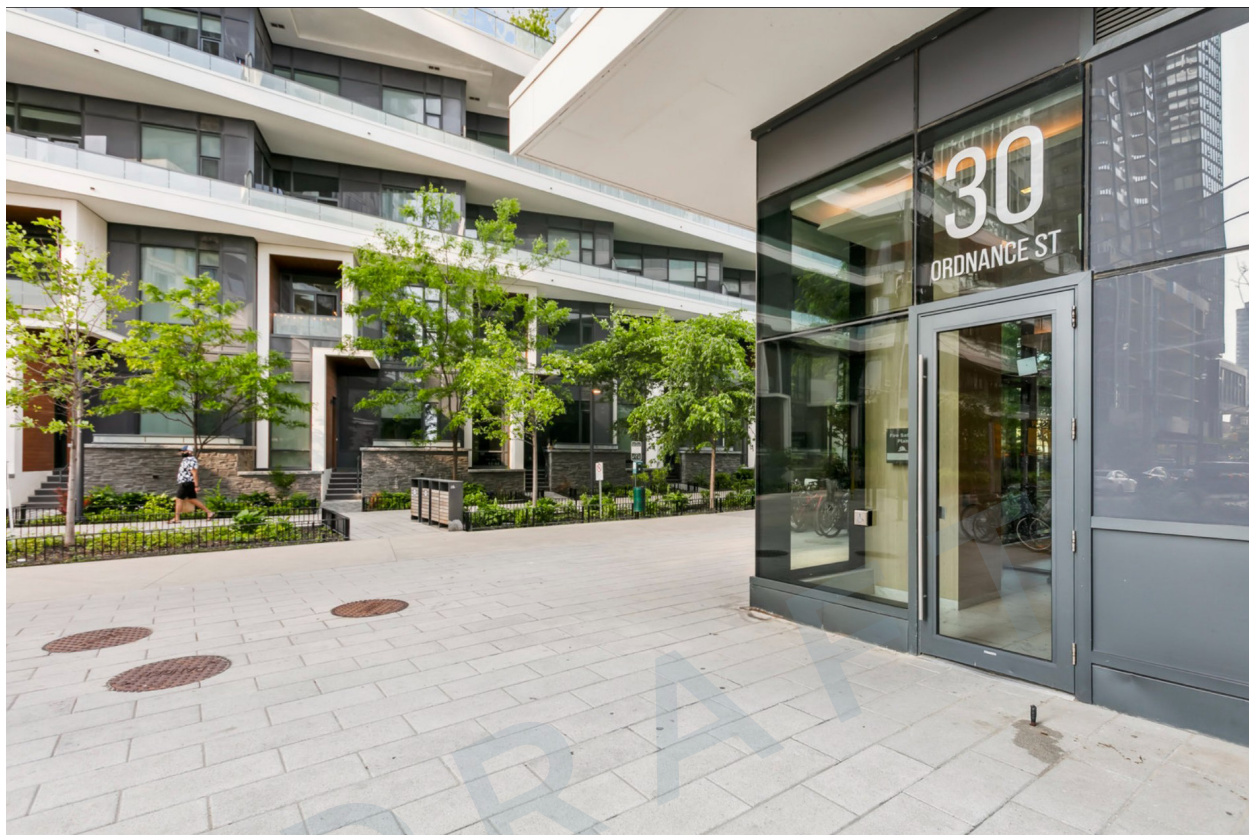


Fig.28: Mid-block connection with active frontages, landscaping, and clear sightlines to promote safety, walkability, and pedestrian comfort.

## 5.3 Parks and Open Space

Parks and open spaces play a vital role in enhancing livability and supporting the physical, mental, and social well-being of residents. Open spaces should be designed to provide a diverse mix of passive and active uses for all ages and abilities, while supporting sustainability, stormwater management, and climate resilience. Relevant standards are outlined in the Comprehensive Engineering and Landscape Manual as well as the City's Accessibility Standards for the Design of Public Spaces.

### Guidelines:

- Public parks should be directly connected to the surrounding street network with high-visibility pedestrian entrances and active building frontages wherever possible.
- Internal park layout should accommodate a variety of uses, including open lawns, play equipment, shaded seating, and circulation paths.
- Site furnishings, including benches, waste receptacles, and lighting, should meet AODA requirements and be constructed from durable, weather-resistant materials.
- Play equipment must meet CSA-Z614 safety standards, be located to minimize noise impacts, and be sited with adequate separation from adjacent residential uses.



Fig.29: Parkette located at key intersection with curved seating and planters to provide a welcoming public gathering space, enhance the streetscape, and support both passive use and pedestrian comfort.



- Landscaping should incorporate a mix of native vegetation, canopy trees, and microclimatic design elements to support year-round comfort, biodiversity, and shade.
- Parks located adjacent to development parcels should promote active frontages and natural surveillance to enhance safety and community interaction.
- Tree planting, shade structures, and seating should be integrated into all parks, with careful species selection to ensure long-term canopy growth.
- Opportunities for public art, community gardens, and Indigenous placemaking elements are encouraged to enhance sense of place and reinforce community identity.



Fig.30: The building edges frame the landscaped area, creating a clear sense of enclosure while maintaining openness to sunlight. Fronting balconies and windows promote natural surveillance and encourage community interaction, with a seamless transition between built form and shared outdoor space.





Fig.31: A well-designed public park with a mix of active play equipment, seating, and shaded areas, illustrating inclusive design, visual connectivity, and integration with the surrounding tree canopy.



Fig.32: A small urban park located at a visible intersection corner, with sculptural lighting and seating that enhances street presence, encourages use, and serves as a community place marker framed by adjacent buildings.



## 5.4 Landscape Integration and Urban Canopy

Landscaping within the public and private realms must be coordinated to support a unified streetscape, while enhancing ecological health, biodiversity, and climate resilience. A strong urban canopy should be established across Sugarbush South to support the City's 40% canopy cover target and improve air quality, stormwater retention, and thermal comfort.

### Guidelines:

- Street trees must be spaced regularly along all public frontages, planted in structural soil or continuous trenches with appropriate setbacks from utilities and buildings.
- In constrained areas, innovative planting techniques such as soil cells, root barriers, or Silva Cells may be required to support tree health and longevity.
- Front yard landscaping, corner lot treatment, and interface zones visible from the public realm must be designed to complement the public realm.
- Native and drought-resistant species are preferred to support biodiversity and long-term sustainability.
- A double row of trees should be planted in key areas such as park edges and locations with wider boulevards.
- Green infrastructure, such as bioswales, permeable pavers, and stormwater-absorbing tree trenches, should be integrated throughout the public and private realm to reduce heat island effects and manage surface runoff.

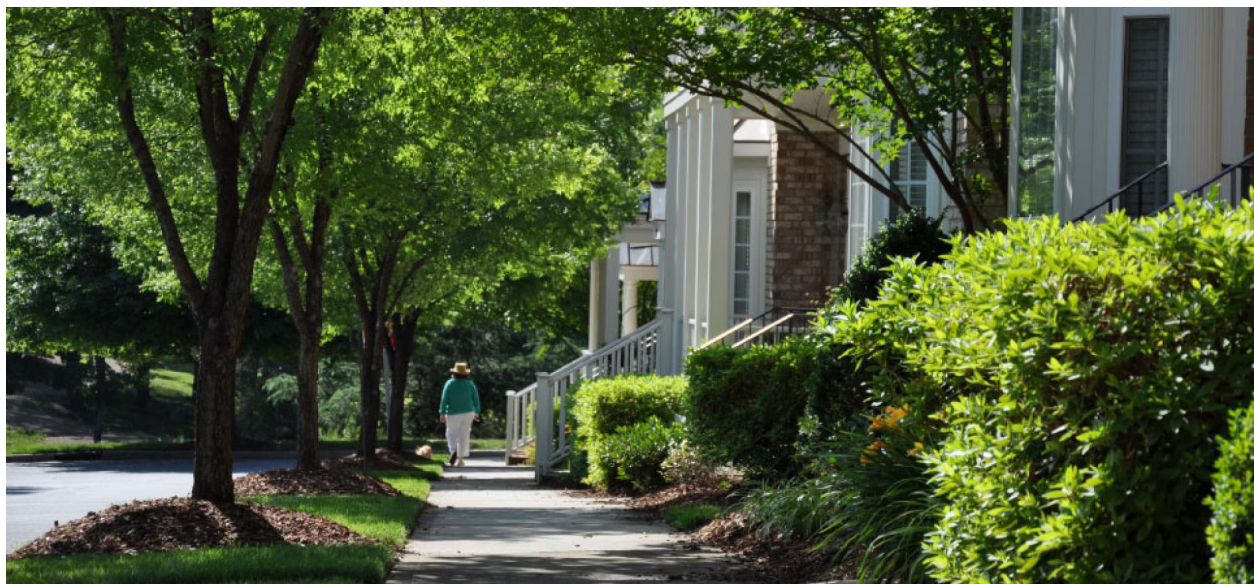


Fig.33: Mature street trees and layered front yard landscaping create a shaded, comfortable pedestrian environment while supporting biodiversity and a seamless transition between the public sidewalk and private dwellings.

## 5.5 Public Art, Identity, and Placemaking

Public realm design must contribute to placemaking by reinforcing Sugarbush South's distinct identity and supporting vibrant, inclusive, and memorable public spaces.

### Guidelines:

- Public art may include murals, sculptures, interpretive signage, or integrated artistic treatments within building façades, paving, or furnishings.
- Cultural expression should reflect local heritage, Indigenous presence, or community narratives, and may be guided through engagement with cultural organizations, artists, and residents.
- Prominent intersections, park entrances, and gateway locations should be designed to include public art or enhanced placemaking elements such as identity markers, special paving, or custom furnishings.
- Placemaking efforts should be coordinated with streetscape design, lighting, material palettes, and furniture selection to create inviting, safe, and contextually appropriate public spaces.
- Public art and placemaking features on private development sites visible from the public realm are encouraged, subject to design review and coordination with City staff.



Fig.34: A sculptural public art piece rooted in Indigenous culture, integrated into the streetscape, serving both as a placemaking element and a celebration of local identity and heritage.





Fig.35: Public art or placemaking features integrated into a mid-block connection.



Fig.36: Integrated public art enhances the building's identity and enriches the streetscape. The abstract mural animates the blank façade, adding visual interest and reinforcing a sense of place and cultural expression.

## 6.0 IMPLEMENTATION

---

### 6.1 Planning and Development Approvals

The Sugarbush South Urban Design Guidelines are a component of the City of Waterloo's Urban Design Manual and are intended to guide development applications within the Sugarbush South neighbourhood. These Guidelines support the Official Plan's intensification and *compatibility* policies and will be used to:

- Evaluate Official Plan Amendments, Zoning By-law Amendments, Minor Variances, and Site Plan applications;
- Shape site design, building form, and public realm improvements during the development approvals process;
- Inform staff reports, conditions of approval, and agreements tied to redevelopment.

All applications within Sugarbush South must demonstrate consistency with these Guidelines and clearly address compatibility, transition, streetscape treatment, and site integration.



